



Palos Verdes Drive N

WESTERN AVENUE CORRIDOR

DESIGN IMPLEMENTATION GUIDELINES

W Summerland Ave



This is a project for the **City of Rancho Palos Verdes** and the **City of Los Angeles** with funding provided by the **Southern California Association of Governments' (SCAG) Compass Blueprint Program**. Compass Blueprint assists Southern California cities and other organizations in evaluating planning options and stimulating development consistent with the region's goals. Compass Blueprint tools support visioning efforts, infill analyses, economic and policy analyses, and marketing and communication programs. The preparation of this report has been financed in part through grant(s) from the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) through the U.S. Department of Transportation (DOT) in accordance with the provisions under the Metropolitan Planning Program as set forth in Section 104(f) of Title 23 of the U.S. Code.

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JACK
IN THE
BOX

O'Reilly
AUT
PART

ROSE
5.4R
Maui
Chicken

SALES
876
WELLNESS
FOOD

Ralphs

Park Western

SALLY

WILSON
MORTGAGE

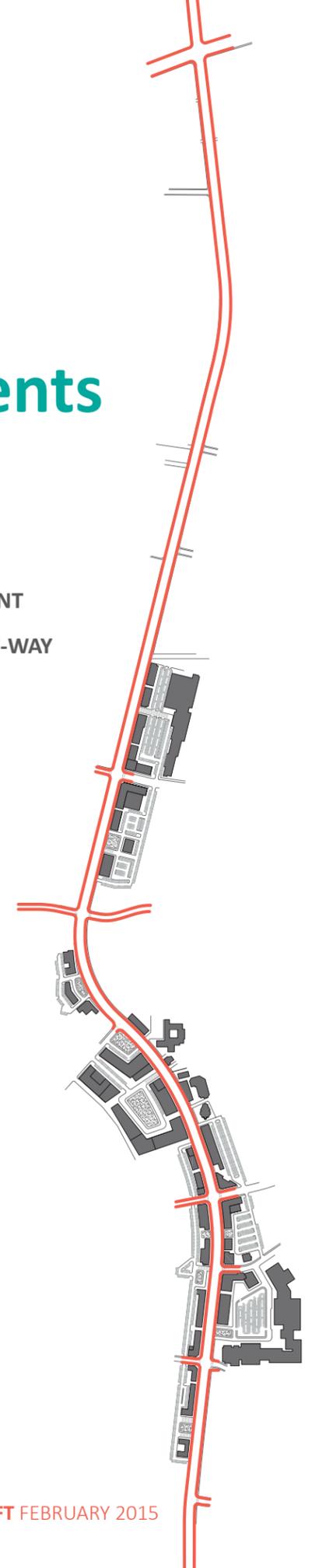
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1

Introduction and Vision

In 2012, the City of Rancho Palos Verdes, funded by Southern California Association of Governments (SCAG), embarked on a community-led effort to improve Western Avenue for residents, businesses, and visitors alike. The Western Avenue Vision Plan document, adopted in 2013, summarized and illustrated the shared vision, ideas, and process that came from that 12-month process.

In 2014, the City of Rancho Palos Verdes, together with the City of Los Angeles, was awarded a second SCAG grant funding the development of Design Guidelines for the implementation of the Corridor Vision.

The significance of this effort, for the City of Rancho Palos Verdes and Los Angeles, and the adjoining communities, cannot be overstated. It will shape Western Avenue for the next generation and its recommendations will impact the quality of life of residents and visitors, the potential of property holdings, the provision of additional amenities and infrastructure, and the overall image of the Avenue.

History

The study area constitutes a small segment of Western Avenue, one of the longest streets in Southern California. At 27.5 miles, it could well host the Los Angeles Marathon with room to spare. It is also the only corridor in the region that connects the mountains (at Griffith Park) to the sea (at White's Point). In its long journey to the bluffs of San Pedro, Western Avenue traverses some of the most iconic neighborhoods and communities in the region, successively adopting their identities, and serving as a lasting symbol of

Southern California's diversity and vitality. The cities that host Western Avenue include Los Angeles, Westmont, Gardena, Torrance, Lomita, and Rancho Palos Verdes, as well as the unincorporated communities of Westmont and West Athens.

Western Avenue has an anachronistic name. Early in the twentieth century it did indeed serve as the western boundary of the city of Los Angeles. The city and the region have, since, decisively blown through the frontier it represented, and Western Avenue today finds itself occupying the heart rather than the periphery of the metropolitan region. It has become the pre-eminent north-south boulevard of the region, and the only one that matches the iconic significance of the region's celebrated east-west boulevards – Sunset, Hollywood, Wilshire, Venice, Pico and Olympic.

Western Avenue is the primary corridor of the South Bay, Peninsula, and San Pedro communities. This study focuses on a two-mile stretch from Palos Verdes Drive North on the north to Peck Park at Summerland Avenue on the south. This segment of Western Avenue has historically provided services, amenities, connectivity, and residential opportunities to the region.

The study corridor, for most of its length, constitutes the municipal boundary between the cities of Rancho Palos Verdes (on the west) and Los Angeles (on the east). It provides a diversity of uses with commercial being concentrated on the south, a mix of commercial and residential uses between Toscanini and John Montgomery Drives, and institutional uses located at the north end. Western Avenue is by no means homogeneous. It provides a multitude of amenities to a multitude of users.

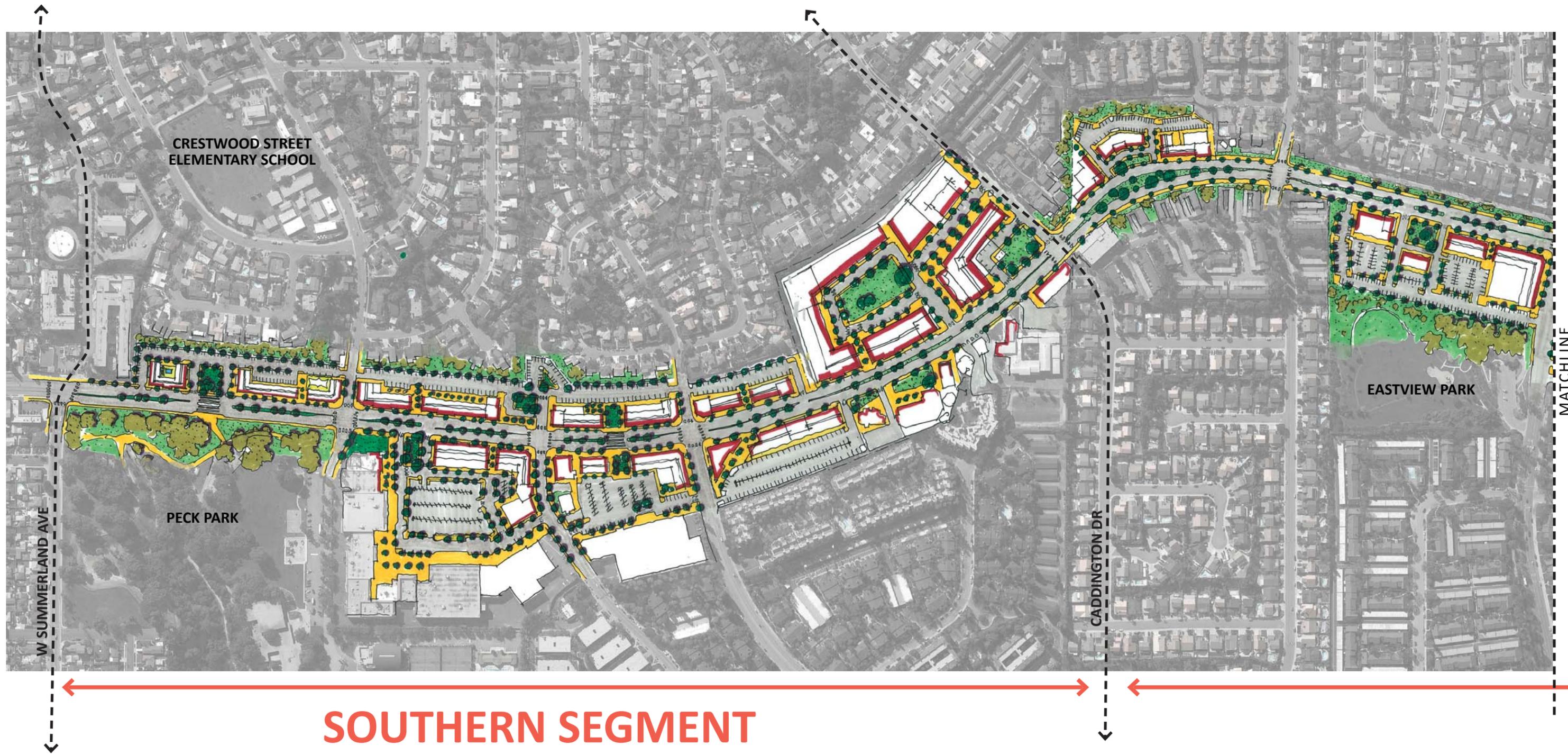
The corridor is, however, dated. Its patterns of development are representative of a time and approach long past. The commercial cluster on the south end of the study area is auto oriented; with a notably poor pedestrian experience. The residential uses in the middle and north segment turn their backs to the street and do not contribute to the street's vitality. Neither commercial nor residential developments would be considered acceptable today. Further, the study corridor lacks special places – plazas, parks, and other hubs of community life. These are essential for a successful, if not great, boulevard.

Outreach

The message heard resoundingly from stakeholders and the community during the Western Avenue Vision Plan effort, was to improve storefronts, quality of the public realm, and perception of the corridor. It was felt that the corridor also needed to create diversity in its retail, access, and mobility options. If the needs of locals were addressed by providing amenities and improving the Avenue's image, surely visitors and business activity would follow thereafter.



Throughout the development of the Design Guidelines, the consultant team met extensively with the City of Rancho Palos Verdes, the City of Los Angeles, Caltrans, and SCAG to reaffirm the vision, and test and refine guidelines to implement the Vision. A Steering Committee was formed, which included members of the original Vision Committee, to help guide the development of guidelines.



SOUTHERN SEGMENT



MIDDLE SEGMENT

NORTHERN SEGMENT



2

Corridor Design

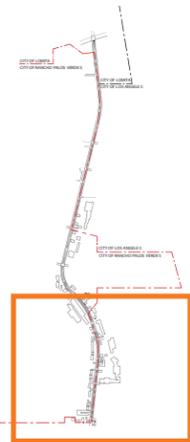
Guidelines and Illustrative Design Sections

2.1 Corridor Design

The core recommendation of the Western Avenue Corridor Vision Plan, as illustrated in Section 1: Introduction and Vision, is to update the nature of development along the corridor and reverse the relationship that buildings and surface parking have with the street.

New developments should be built along the property line creating a strong street wall. Surface parking should not be located adjacent to the sidewalk and should not serve as the arrival experience of the corridor. With buildings located at the property edge on the sidewalk, active, visitor-serving uses should be located at ground level.

Guidelines for each of the three segments of the corridor, are provided on the following pages.



2.2 Guidelines for the Southern Segment

SOUTHERN SEGMENT, SUMMERLAND AVENUE TO CADDINGTON DRIVE

The Southern Segment of the corridor, from Summerland Avenue to Caddington Drive, is the commercial heart of the study area, and its experience and image needs updating to sustain its success. The relationship that buildings and surface parking have with the street should be reversed, so that new

developments is built along the property line, creating a strong street wall. Parking should be located at the rear of the parcel and/or consolidated in strategically located structures. Active, visitor-serving uses should be located at the ground level.

The following guidelines apply:

1. New development should be built at the property line creating a strong street wall.
 - Parking should be located at the rear of the parcel and/or consolidated in strategically located parking garage structures.

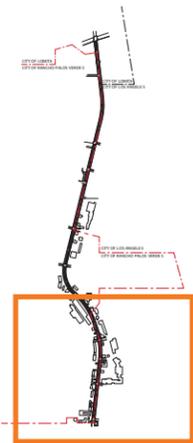
- Active, visitor-serving uses, typically commercial, should be located at the ground level with direct access to the sidewalk.
 - See [Section X-X] for additional guidelines.
2. Along both the west and east sides of the ROW, street trees and flowering plants should be incorporated into the design of the streetscape.
 - Sidewalks widths should be 15-feet at a minimum with a streetscape, including landscape, furniture, lighting, and pedestrian amenities, that promotes a vibrant street life.
 - Generally, landscape shall be more formal and durable, in response the higher volume of pedestrian activity.
 - Street parking should remain. [Approach to be confirmed: It may be converted into bump outs at corners or bike lane.]
 3. Median landscaping should include palm trees as well as continuous landscaping, to create a consistent rhythm of trees and establish branding and identity for the corridor.
 - Low planting should be used strategically to preserve views.
 4. Opportunities to create outdoors spaces and special places should be a priority in all development projects.
 5. Standard cobra street lighting should be replaced, and supplemented with decorative pedestrian lighting, to strengthen branding and identity for the corridor. See [Section X-X] for additional guidelines.
 6. Low planting should be installed to soften parking lots.



- A** Commercial development should be brought to the street edge to create a strong street wall, and incorporate signage, canopies, and other visual elements to enliven street life.
- B** The design of the storefronts and streetscape should facilitate pedestrian activity, and incorporate elements such as outdoor seating areas for restaurants and cafes.
- C** Bicycle parking infrastructure should be located at intervals along the streetscape, to encourage visitors to bike between destinations.
- D** Landscaping adjacent to pedestrian traffic should be durable, distinct, and colorful.



Conceptual sketch of character envisioned for the Southern Segment, looking north on Western Ave at Capitol Drive.



Southern Segment

OPTION A- With bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1 Provide a minimum 2ft. "convenience strip" for pedestrian egress. See Section X-XX.
- 2 Locate trees within streetscape planting zones, and space approximately every 30ft.
- 3 Maintain a minimum distance of 10ft. between streetscape planting zones to accommodate street furniture and utilities. See Section X-XX.

Private Realm

- 1 Facilitate pedestrian activity on sidewalk by incorporating elements such as outdoor seating areas for restaurants and cafes. See Section X-XX.

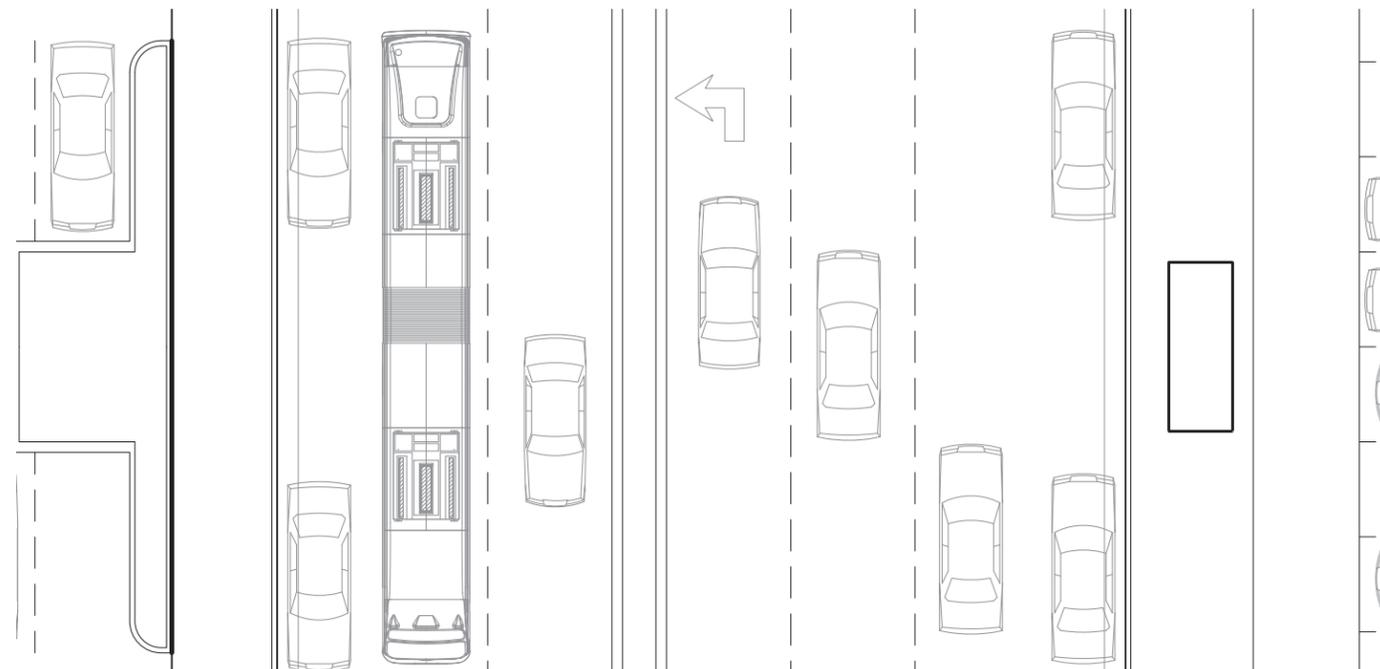


Fig X-XX
Southern Segment
Existing typical street plan

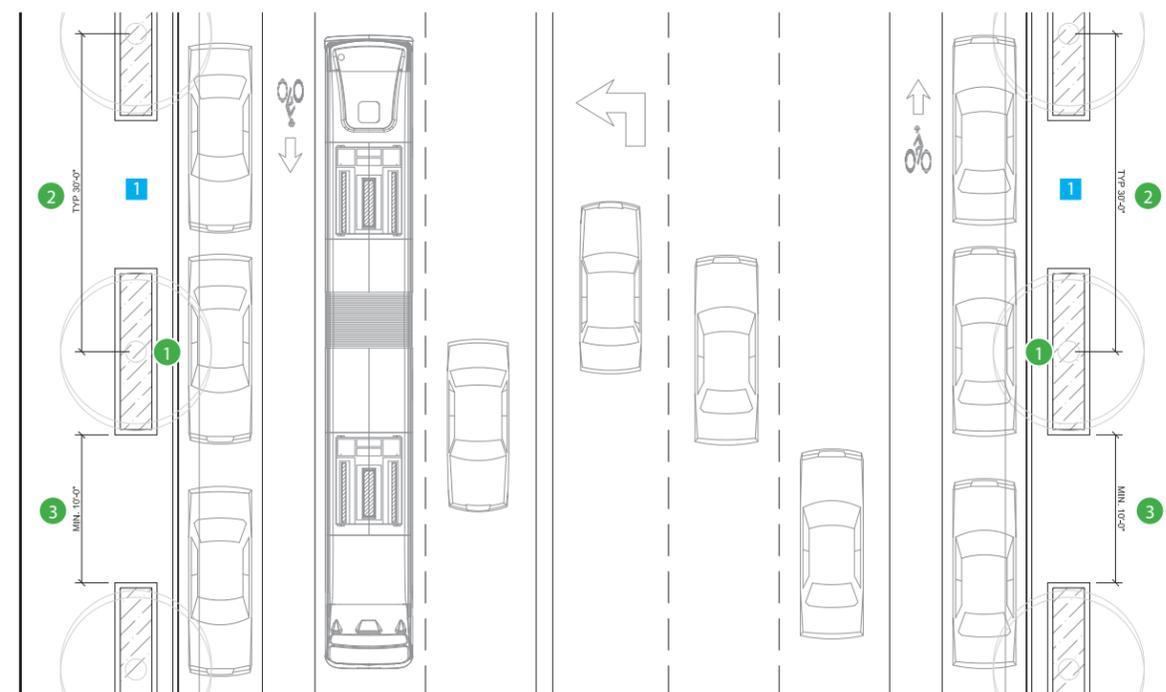
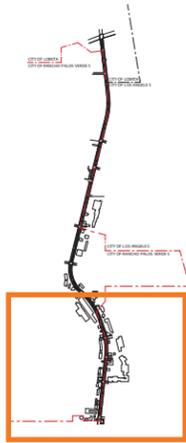


Fig X-XX
Southern Segment
Option A - With bikeway
Recommended typical street plan



Southern Segment

OPTION A- With bikeway

Public Realm

- 1 Improve median planting. When left turn pocket occurs, use median nose or striping. See X-XX.
- 2 Maintain existing on-street parking and add new bikeway. See X-XX.
- 3 Reduce width of travel lanes as noted.
- 4 Add new streetscape planting. See X-XX.
- 5 Relocate existing utility poles below-grade and add new street and pedestrian lighting. See X-XX.

Private Realm

- 1 For new construction, set building 15 ft. from curb to accommodate an expanded sidewalk. See X-XX.
- 2 Design building facades and streetscape to facilitate pedestrian-oriented activity and enliven street life. See X-XX.

Fig X-XX
Southern Segment
Existing typical street section

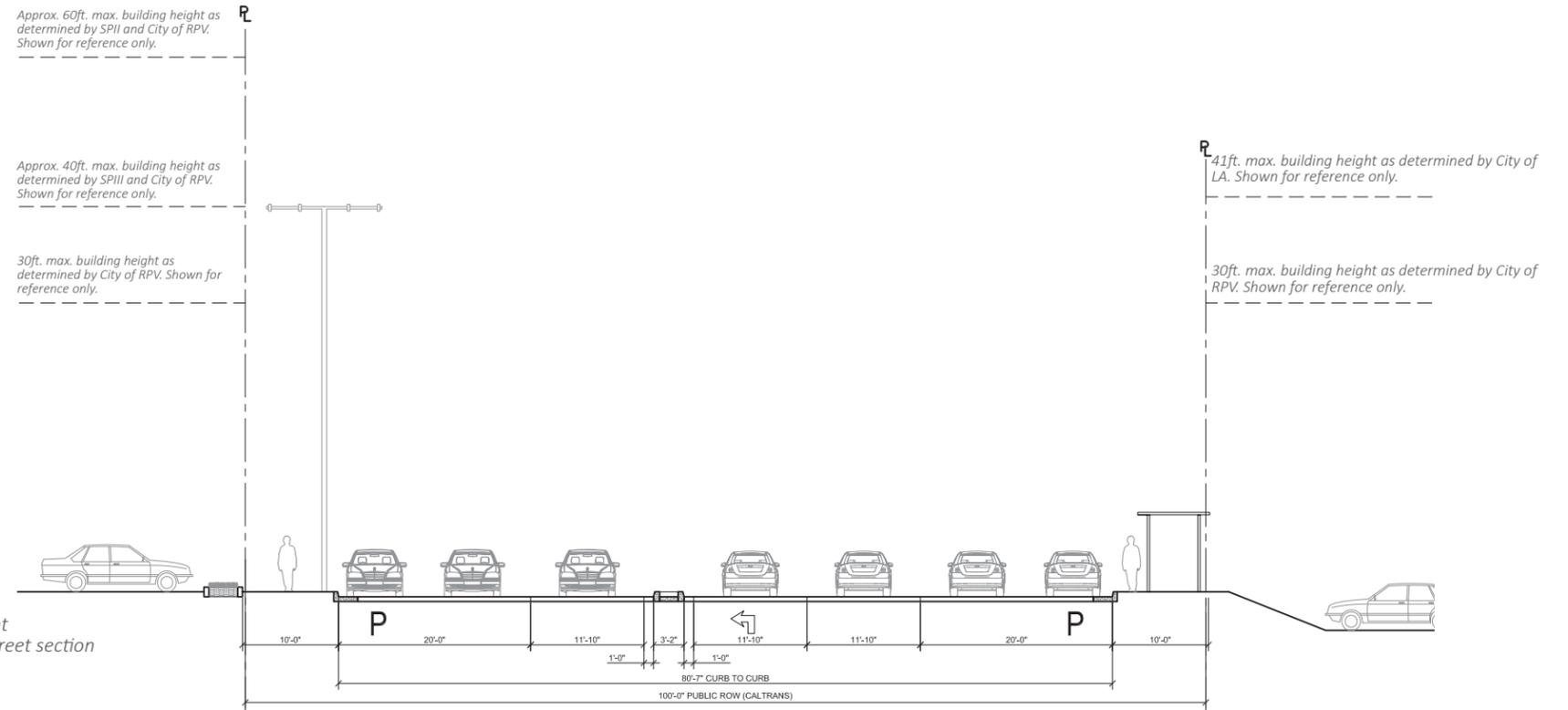
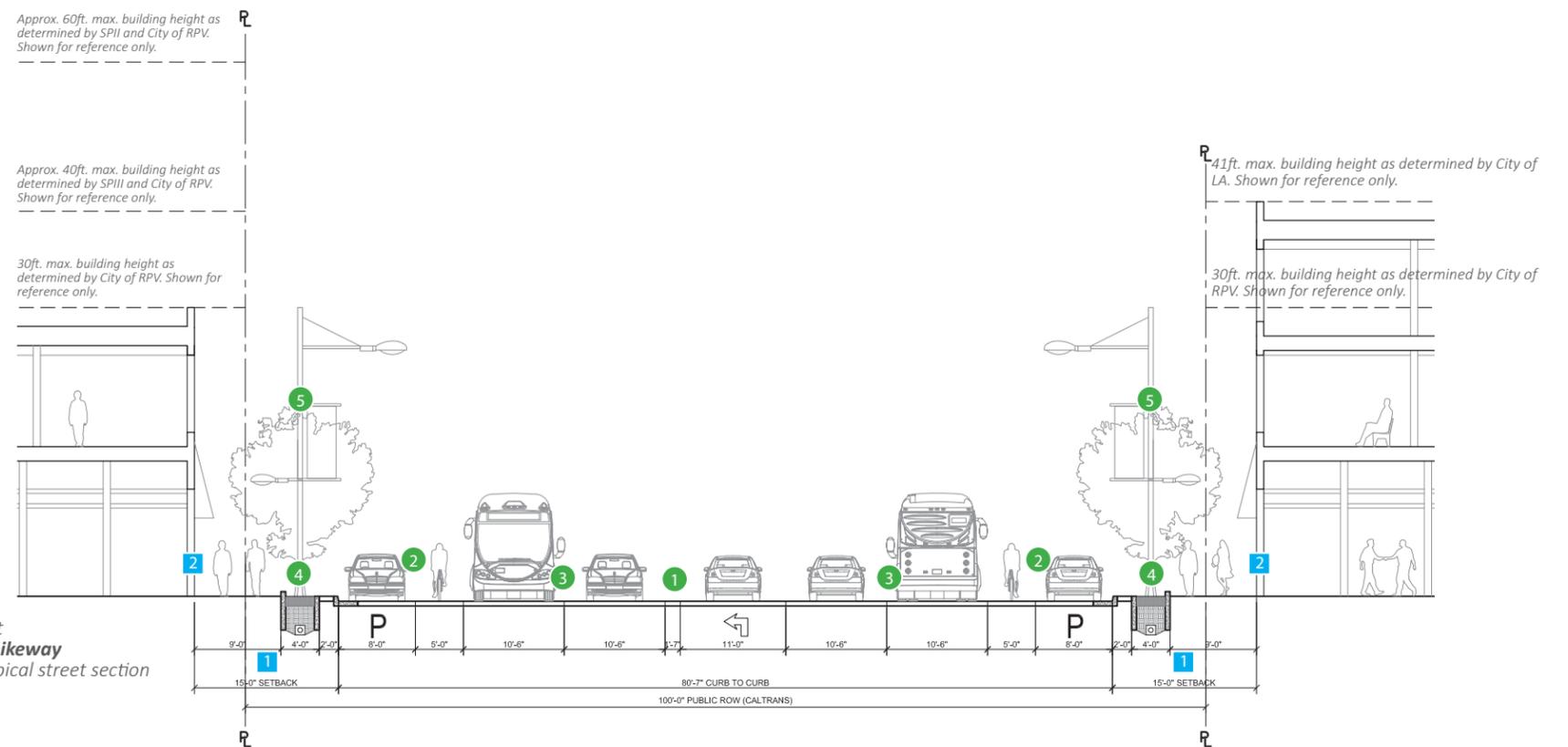
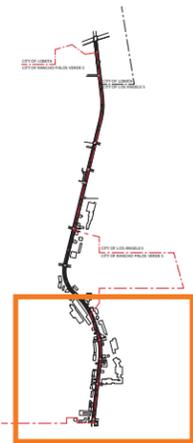


Fig X-XX
Southern Segment
Option A - With bikeway
Recommended typical street section





Southern Segment

OPTION B- No bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1 Same as Option A.
- 2 Same as Option A.
- 3 Same as Option A.

Private Realm

- 1 Same as Option A.

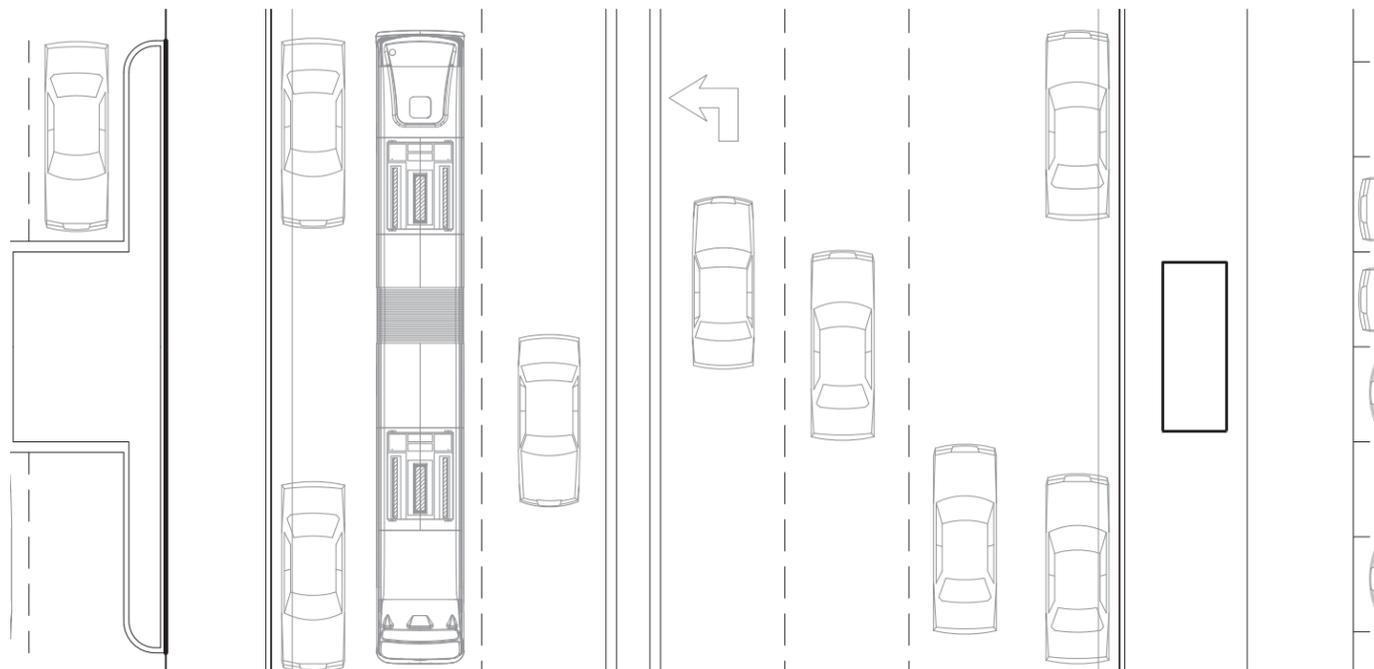


Fig X-XX
Southern Segment
Existing typical street plan

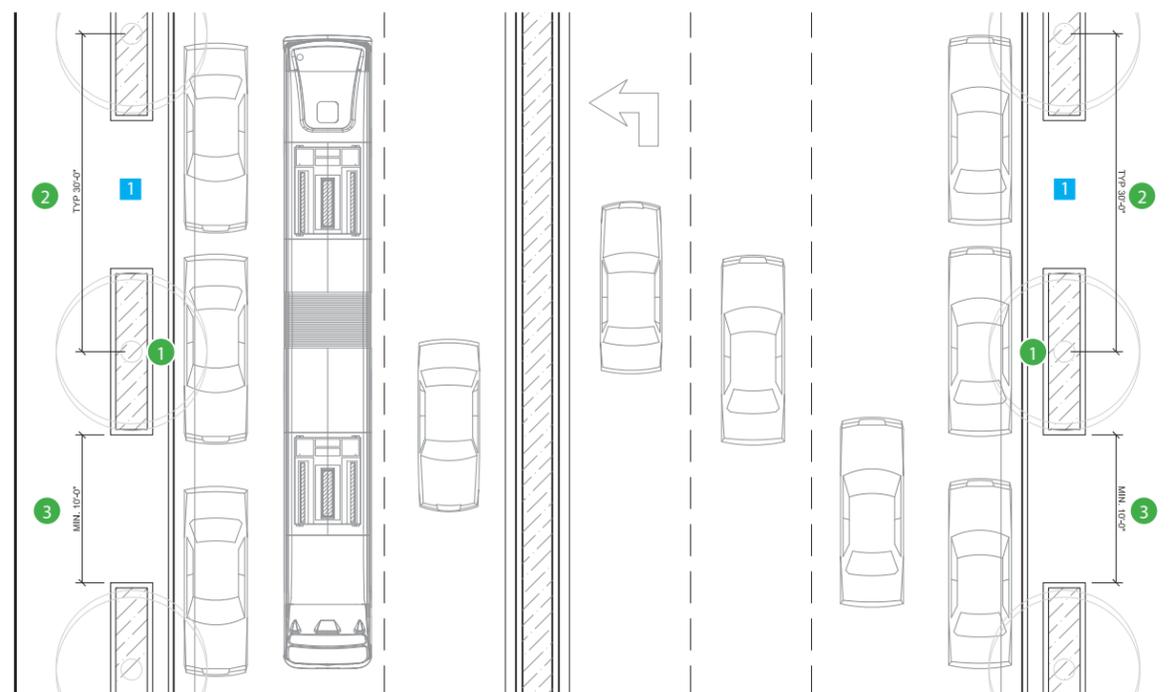


Fig X-XX
Southern Segment
Option B - No bikeway
Recommended typical street plan



Southern Segment

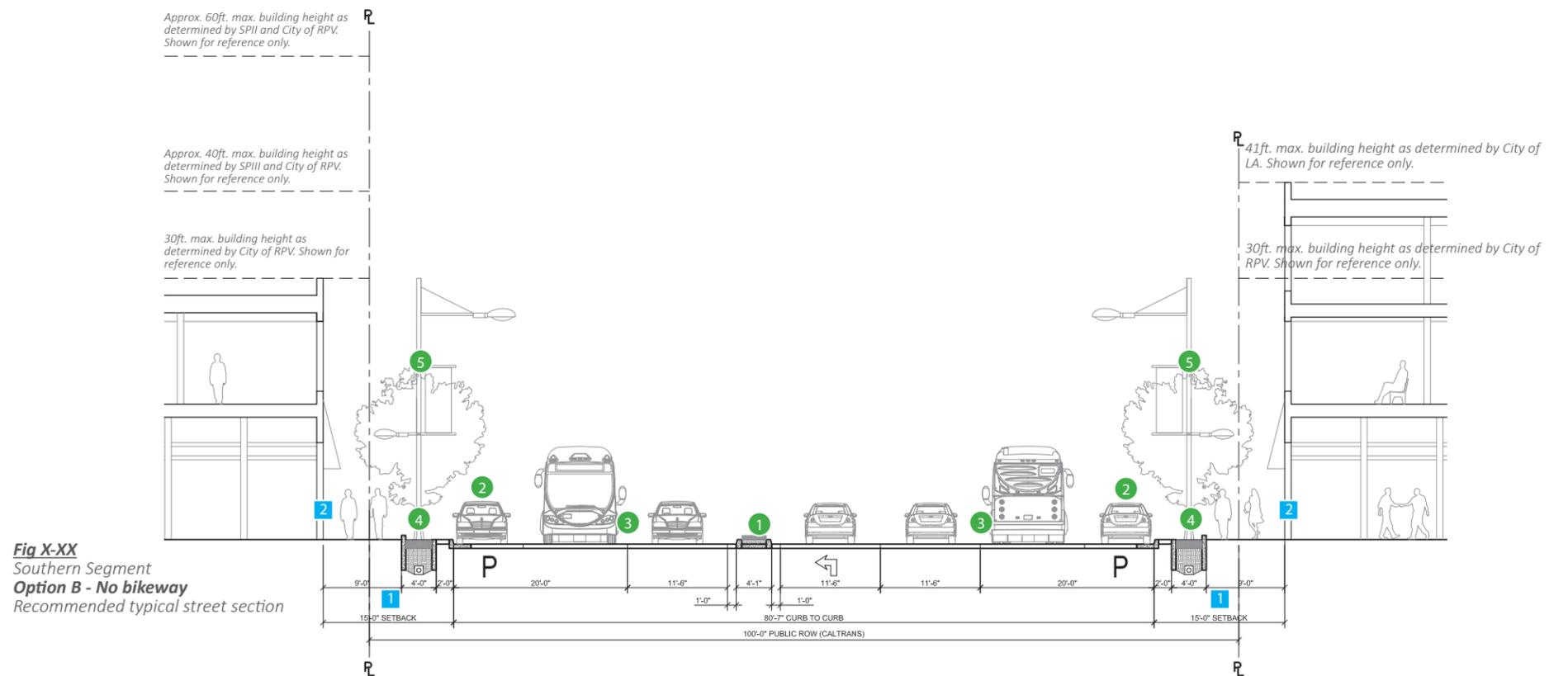
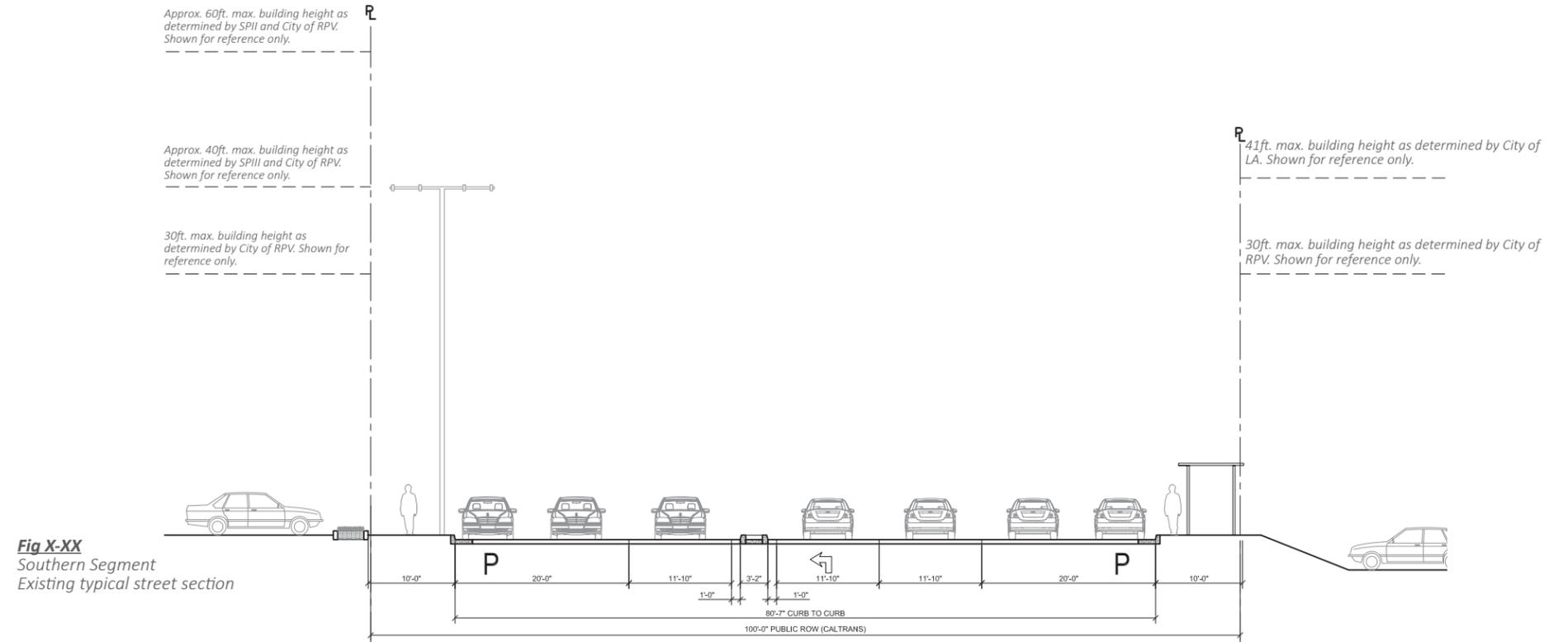
OPTION B- No bikeway

Public Realm

- 1 Same as Option A.
- 2 Maintain existing on-street parking.
- 3 Same as Option A.
- 4 Same as Option A.
- 5 Same as Option A.

Private Realm

- 1 Same as Option A.





2.3 Guidelines for the Middle Segment

MIDDLE SEGMENT, CADDINGTON AVENUE TO JOHN MONTGOMERY DRIVE

The east side of the street (primarily commercial) will receive similar improvements as the southern segment. The west side of the street (primarily residential) presents a challenge with regards to activation. With backyards of homes facing

Western Avenue, there is little opportunity to reconfigure or activate the streetscape. As a result, improvements will mainly be cosmetic, expanding the sidewalk, improving the cinder block wall, and adding streetscape, and street furniture, and landscaping.

The following guidelines apply:

1. At the east side of the street, streetscape improvements should be designed to extend the character of the Southern Segment.
2. Along the west side of the ROW, the following streetscape improvements should be made:
 - Cosmetic improvements should be made to soften the cinder block wall of the residential backyards. A consistent treatment should be used along the length of the corridor, utilizing one of the options identified in [Section X-X].
 - The sidewalk should be widened, eliminating the on-street parking, and adding street trees and a bioretention swale.
 - Parkway landscaping should be more organic and native in character.
3. Along the east side of the ROW, street trees and flowering plants should be incorporated into the design of the streetscape. Generally, landscape shall be more formal and durable, in response the higher volume of pedestrian activity.
4. Median landscaping should be refreshed and

supplemented with palm trees, as well as continuous landscaping, to create a sense of continuity along the corridor.

5. Where possible, existing mature trees should be incorporated into the design of medians and/or parkways ,and used to strengthen branding and identity for the corridor.
6. Utility lines should be moved underground to relieve visual clutter.
7. Decorative street lighting should be added to increase safety and strengthen branding and identity for the corridor.



- A** Commercial development should be brought to the street edge, and incorporate signage, canopies, and other visual elements to enliven street life.
- B** Continuously landscaped medians and street trees should beautify the street, make for a cohesive street character, and aid in storm-water management.
- C** Dedicated bike lanes should be designed to facilitate cycling along the corridor.



Conceptual sketch of character envisioned for the Middle Segment, looking north on Western Ave at Westmont Drive.



MIDDLE SEGMENT

OPTION A- With bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1e On east, provide a minimum 2ft. "convenience strip" for pedestrian egress. See Section X-XX.
- 2 Locate trees within streetscape planting zones, and space approximately every 30ft.
- 3w On west, maintain a maximum distance of 5ft. between streetscape planting zones to accommodate street furniture and utilities. See Section X-XX.
- 3e On east, maintain a minimum distance of 10ft. between streetscape planting zones to accommodate street furniture and utilities. See Section X-XX.

Private Realm

- 1 Facilitate pedestrian activity on sidewalk by incorporating elements such as outdoor seating areas for restaurants and cafes. See Section X-XX.

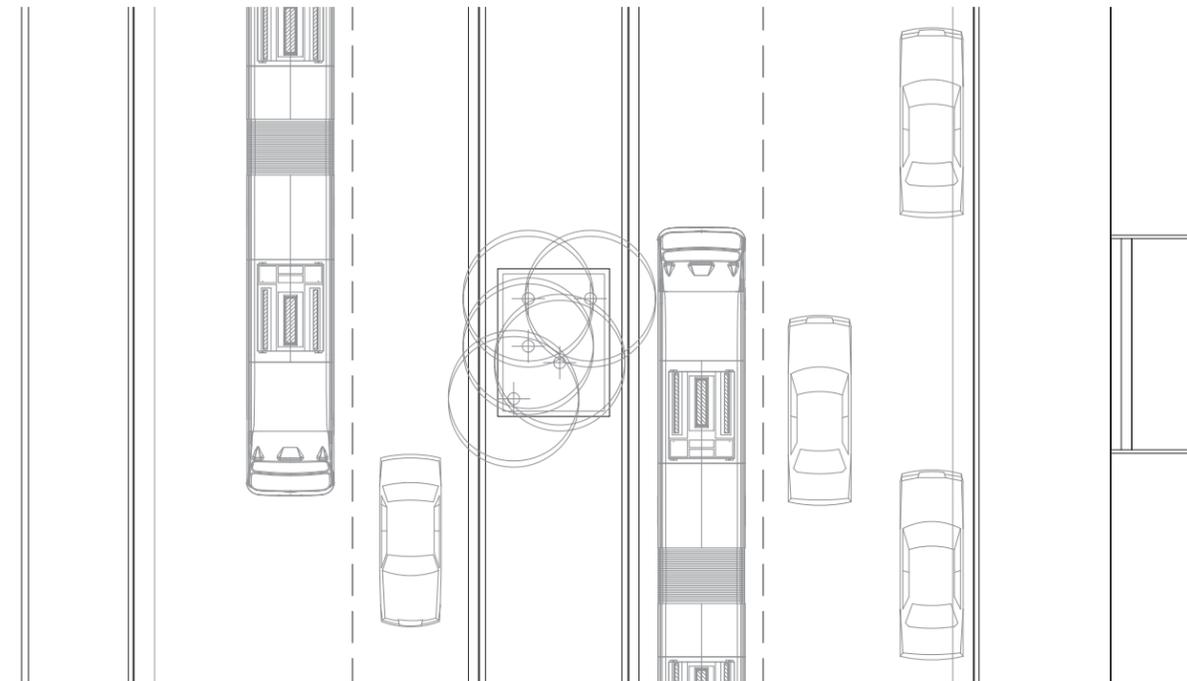


Fig X-XX
Middle Segment
Existing typical street plan

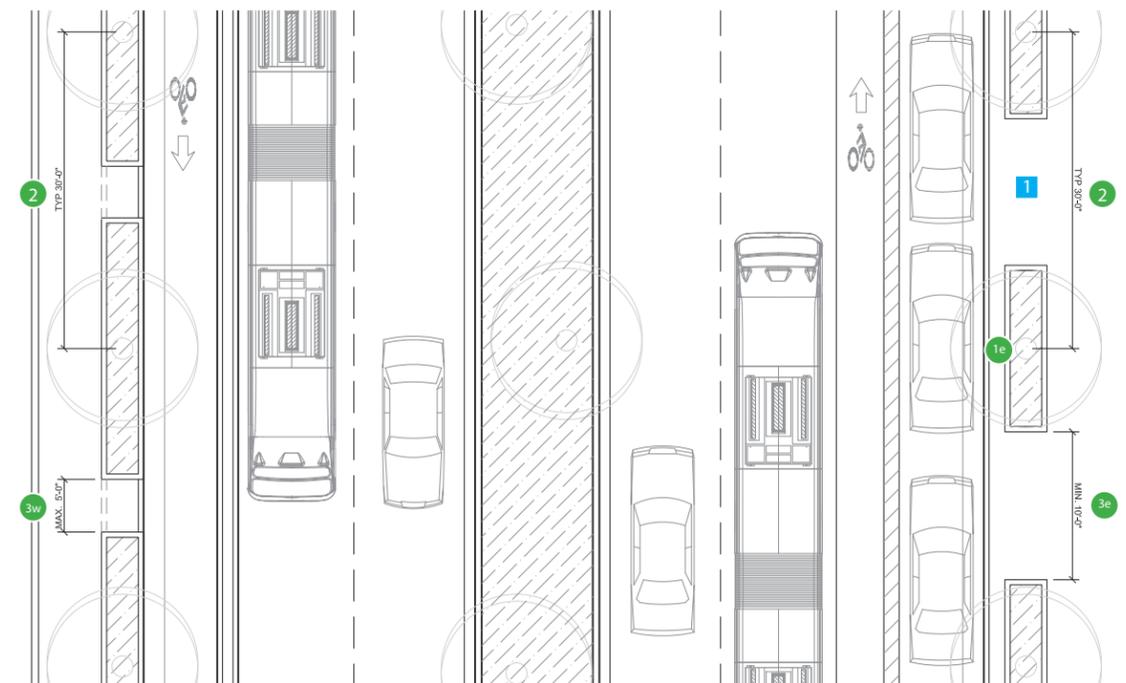


Fig X-XX
Middle Segment
Option A - With bikeway
Recommended typical street plan



MIDDLE SEGMENT

OPTION A- With bikeway

Public Realm

- 1 Improve median planting. When left turn pocket occurs, use median nose or striping. See Section X-XX.
- 2w On west, remove existing on-street parking and replace with new bikeway. See Section X-XX.
- 2e On east, maintain existing on-street parking and add new bikeway. See Section X-XX.
- 3 Reduce width of travel lanes as noted.
- 4w On west, add new streetscape planting. See Section X-XX.
- 4e On east, add new streetscape planting with pedestrian egress "convenience strip." See Section X-XX.
- 5 Relocate existing utility poles below-grade and add new street and pedestrian lighting. See Section X-XX.

Private Realm

- 1 For new construction, set building 15 ft. from curb to accommodate expanded sidewalk. See Section X-XX.
- 2 Design building facades and streetscape to facilitate pedestrian-oriented activity and enliven street life. See Section X-XX.

FOR CITY OF RANCHO PALOS VERDES (RPV) ONLY

- RPV 1 Encourage improvements to existing residential cinder block wall by using planting to "soften" its appearance. See Section X-XX.
- RPV 2 Encourage landscape improvements to existing residential backyard slopes. See Section X-XX.

Fig X-XX
Middle Segment
Existing typical street section

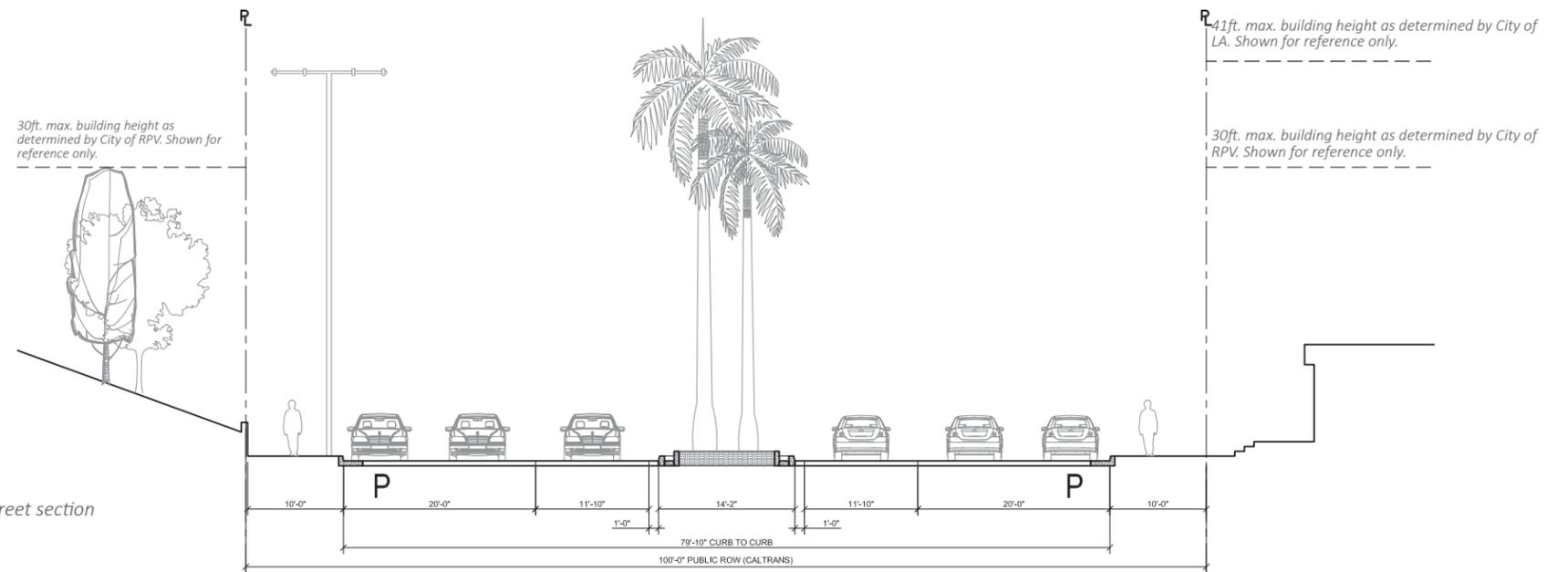
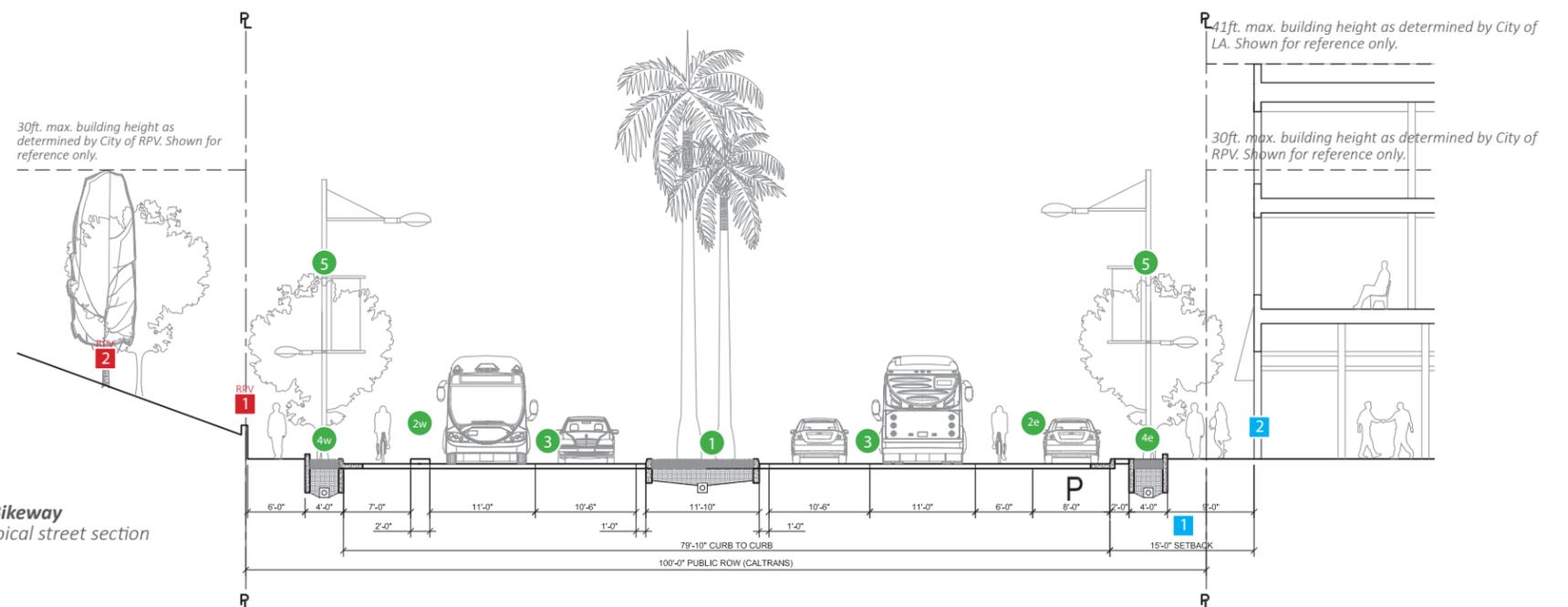


Fig X-XX
Middle Segment
Option A - With Bikeway
Recommended typical street section





MIDDLE SEGMENT

OPTION B- No bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1e Same as Option A.
- 2e On east, locate trees within streetscape planting zones and space approximately 30ft. apart.
- 2w On west, locate trees within planted curb extensions when they occur. See Section X-XX.
- 3e Same as Option A.

Private Realm

- 1 Same as Option A.

Fig X-XX
Middle Segment
Existing typical street plan

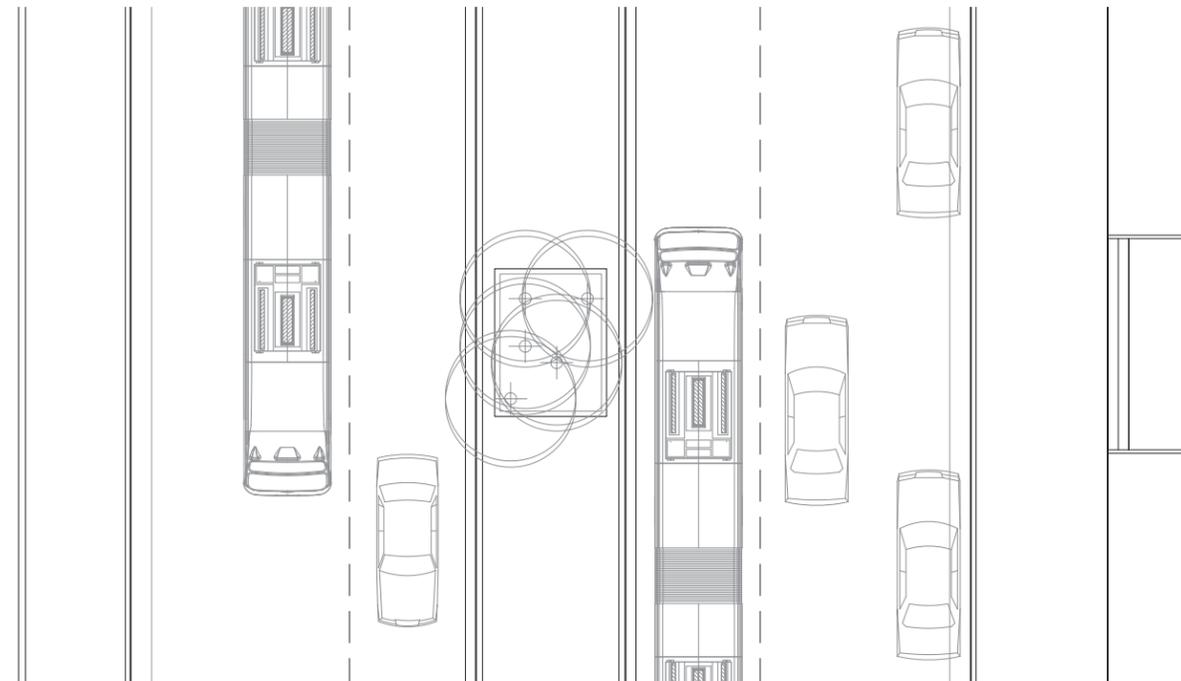
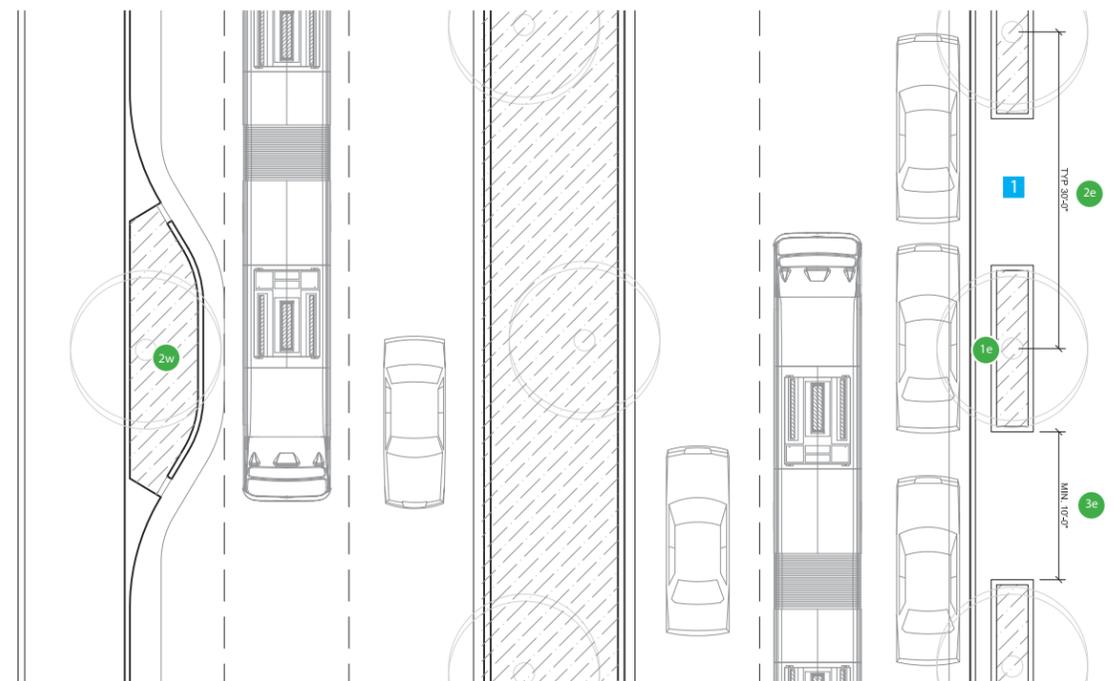


Fig X-XX
Middle Segment
Option B - No bikeway
Recommended typical street plan





MIDDLE SEGMENT

OPTION B- No bikeway

Public Realm

- 1 Same as Option A.
- 2w On west, remove existing on-street parking and add planted curb extensions spaced approximately every 100ft. See Section X-XX.
- 2e Maintain existing on-street parking.
- 3 Same as Option A.
- 4e Same as Option A.
- 5 Same as Option A.

Private Realm

- 1 Same as Option A.
- 2 Same as Option A.

FOR CITY OF RANCHO PALOS VERDES (RPV) ONLY

- 1 Same as Option A.
- 2 Same as Option A.

Fig X-XX
Middle Segment
Existing typical street section

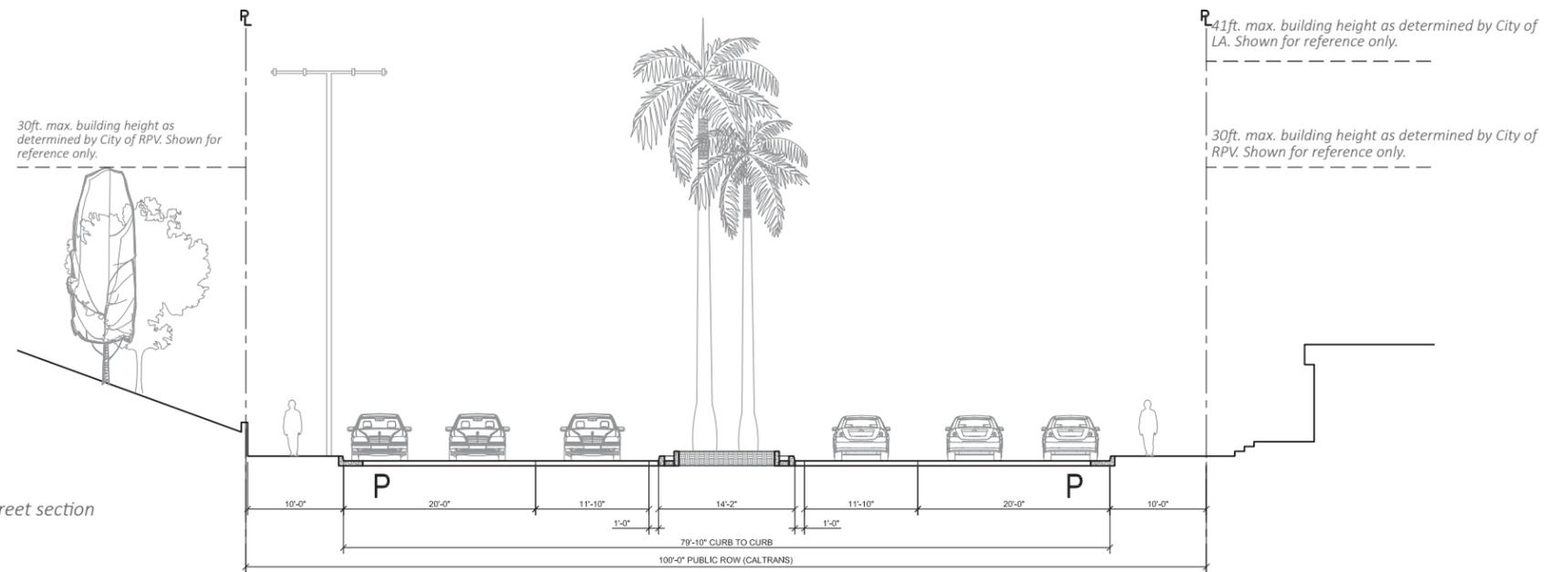
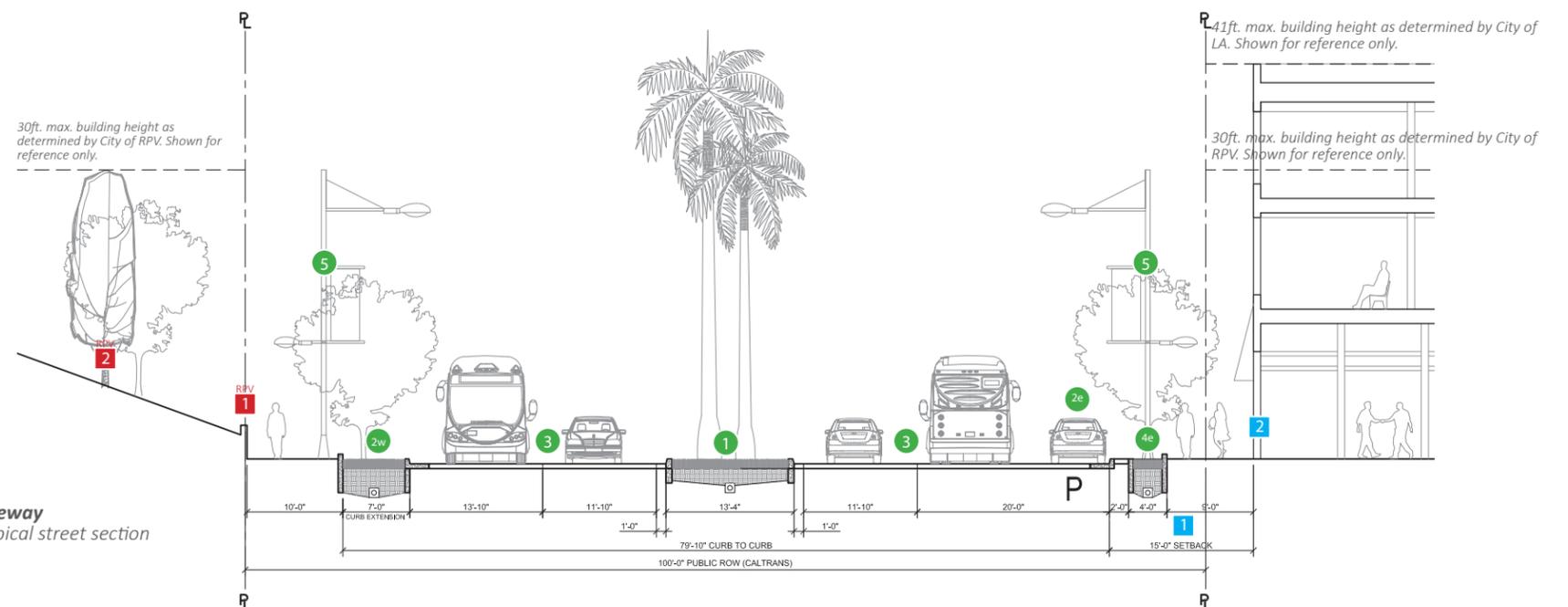


Fig X-XX
Middle Segment
Option B - No bikeway
Recommended typical street section





2.4 Guidelines for the Northern Segment

NORTHERN SEGMENT, JOHN MONTGOMERY DRIVE TO PALOS VERDES DRIVE

The east side of the street is entirely occupied by the fuel storage infrastructure of the Defense Fuel Support Point (DFSP) San Pedro. This facility, for all practical purposes, is here for perpetuity. The west

side of the street is entirely occupied by Green Hills Memorial Park. This facility too is here for perpetuity. Given that the east and west side of the streets will never house active, visitor-serving uses the experience of this segment of the study area is decidedly auto-oriented.

Monumental public art, scaled to be visible to auto users, can be located on the street edges (in partnership with the Cemetery and DFSP). Like on the west side of the middle segment, here too, the sidewalks on both the east and west side of the street should be widened and landscaped.

The following guidelines apply:

1. At the west side of the ROW, buffer planting should be used to soften the high cinder block wall of the cemetery.
 - [Add guidelines for buffer planting here]
2. Median and parkway landscaping should include colorful trees, flowers, and palms to create an entry gateway into the corridor.
3. Parkway landscaping should include the use of bioretention swales.
4. The Cities of Rancho Palos Verdes and Los Angeles should explore opportunities to partner with the Cemetery and DFSP to install public art in the northern segment of the corridor.
 - [Figure X-X] illustrates planned locations for the installation of public art along the corridor.

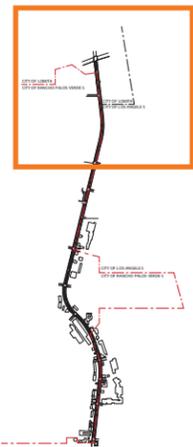
- Public art should be installed within the public ROW, along the Western Avenue Street edge, and carefully integrated with the streetscape and landscaping.
 - Public art should be of a monumental scale, to be visible to vehicular users.
5. A gateway element announcing the arrival southward into the commercial heart of the study corridor.



- A** Buffer planting should be used to soften the high cinder-block wall of the Green Hills Cemetery. Landscaping should incorporate bioretention swales to manage stormwater.
- B** Dedicated bike lanes should connect to the recreational bike path looping the Peninsula.
- C** Median and parkway landscaping should include colorful trees, flowers, and palms to create a distinctive north gateway to the corridor.



Conceptual sketch of character envisioned for the Northern Segment, looking south on Western Ave at Green Hills Cemetery.



NORTHERN SEGMENT

OPTION A- With bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1w Locate street trees within streetscape planting zones and space approximately every 60ft. See Section X-XX.
- 1e Maintain a maximum distance of 5ft. between streetscape planting zones to accommodate street furniture and utilities. See Section X-XX.

Private Realm

- 1 Not applicable.

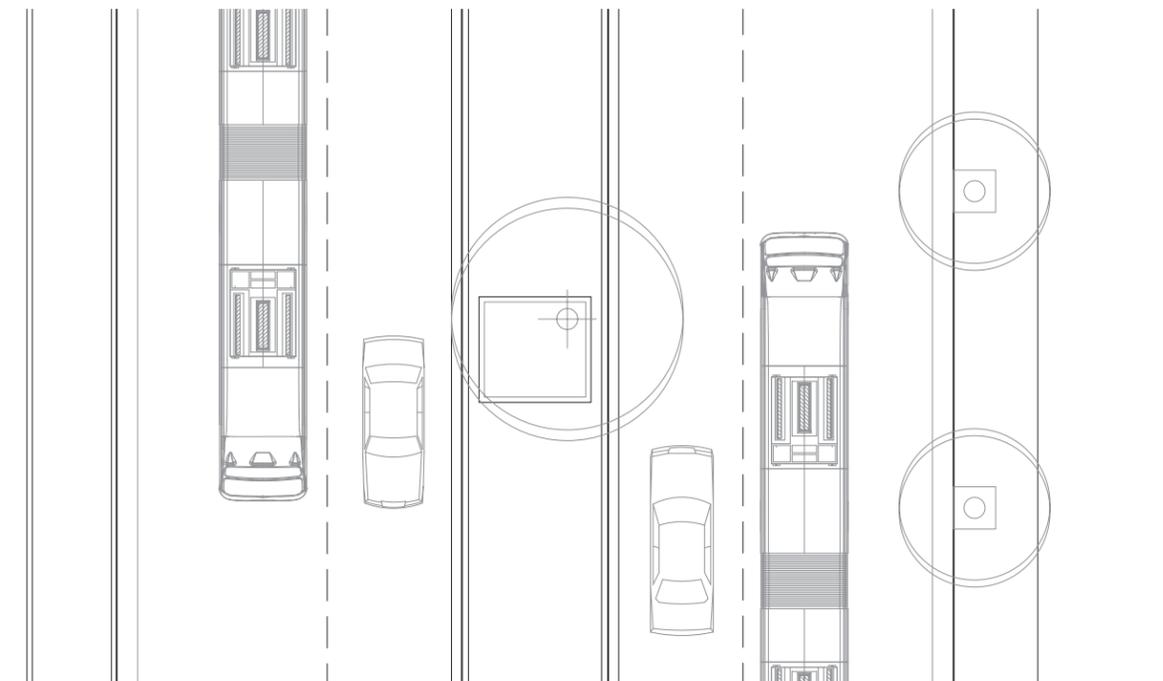


Fig X-XX
Northern Segment
Existing typical street plan

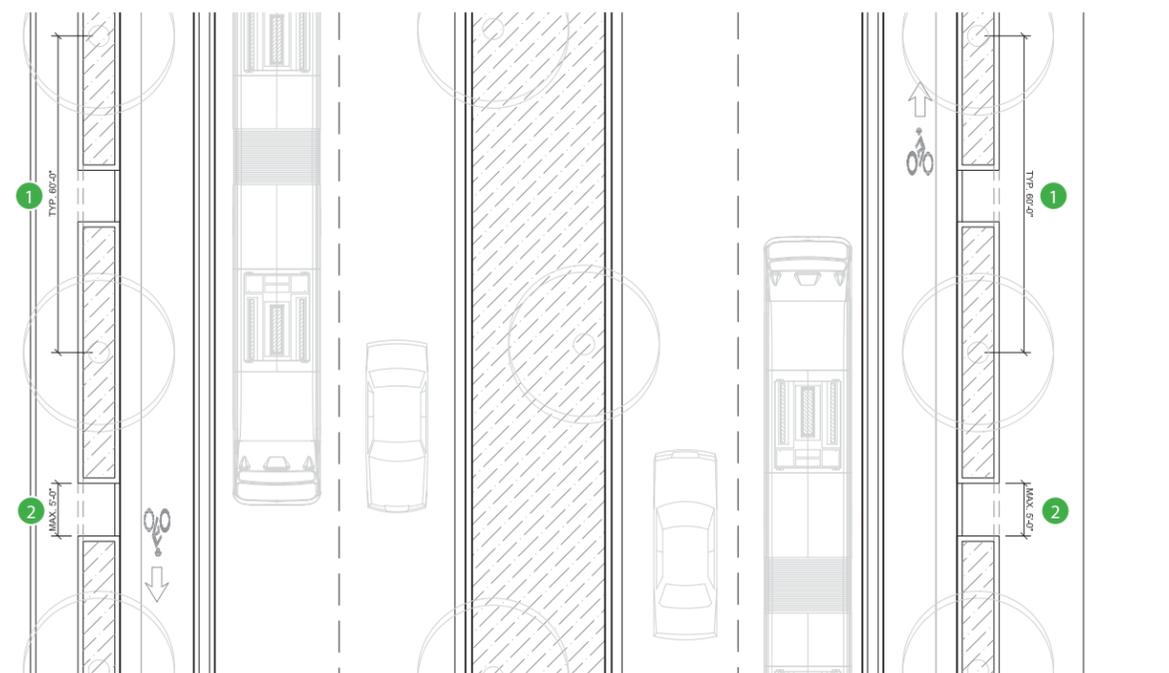


Fig X-XX
Northern Segment
Option A - With bikeway
Recommended typical street plan



Northern Segment

OPTION A- With bikeway

Public Realm

- 1 Improve median planting. When left turn pocket occurs, use median nose or striping. See Section X-XX.
- 2 Remove existing on-street parking and add new bikeway. See Section X-XX.
- 3 Reduce width of travel lanes as noted.
- 4 Add new sidewalk planting. See Section X-XX.
- 5 Relocate existing utility poles below-grade and add new street and pedestrian lighting. See Section XX.
- 6 Incorporate temporary or permanent “road-side” artwork and signage in the median. See Section X-XX.

FOR CITY OF RANCHO PALOS VERDES (RPV) ONLY

- 1 Encourage improvements to existing cemetery cinder block wall by using planting to “soften” its appearance. See Section X-XX.

FOR CITY OF LOS ANGELES (LA) ONLY

- 1 Expand sidewalk eastward to a minimum width of 5ft or greater up to property line.

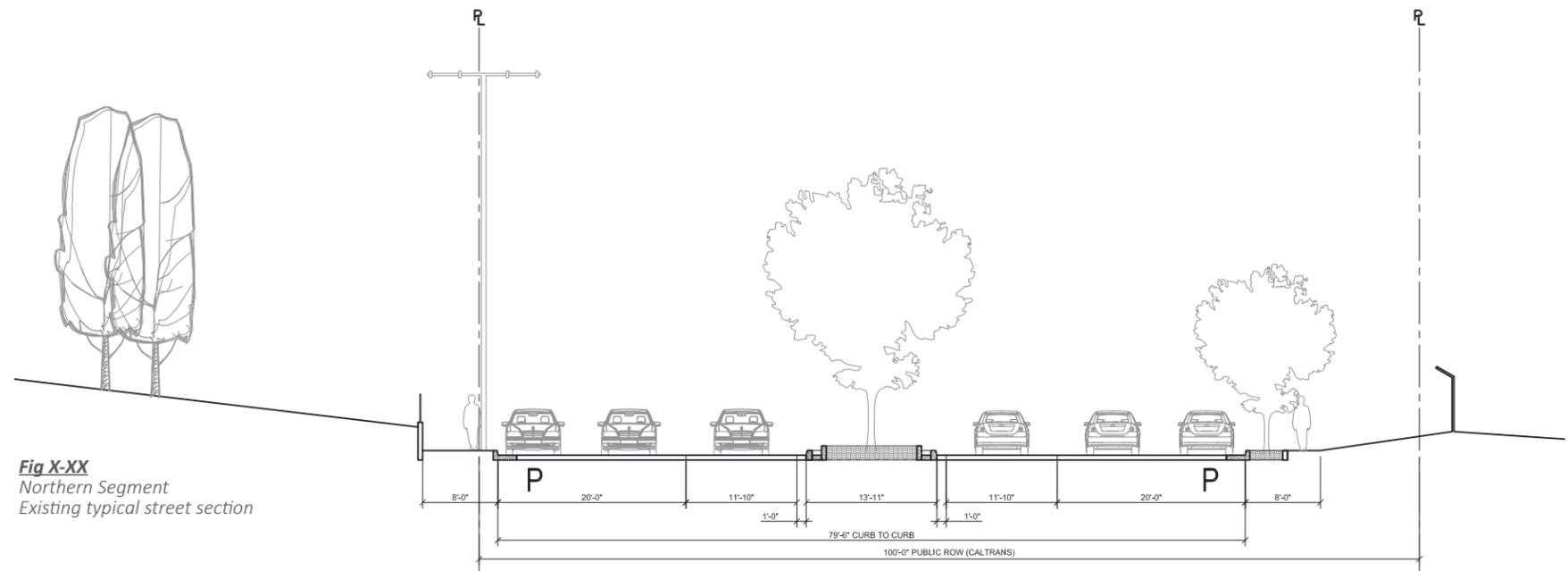


Fig X-XX
Northern Segment
Existing typical street section

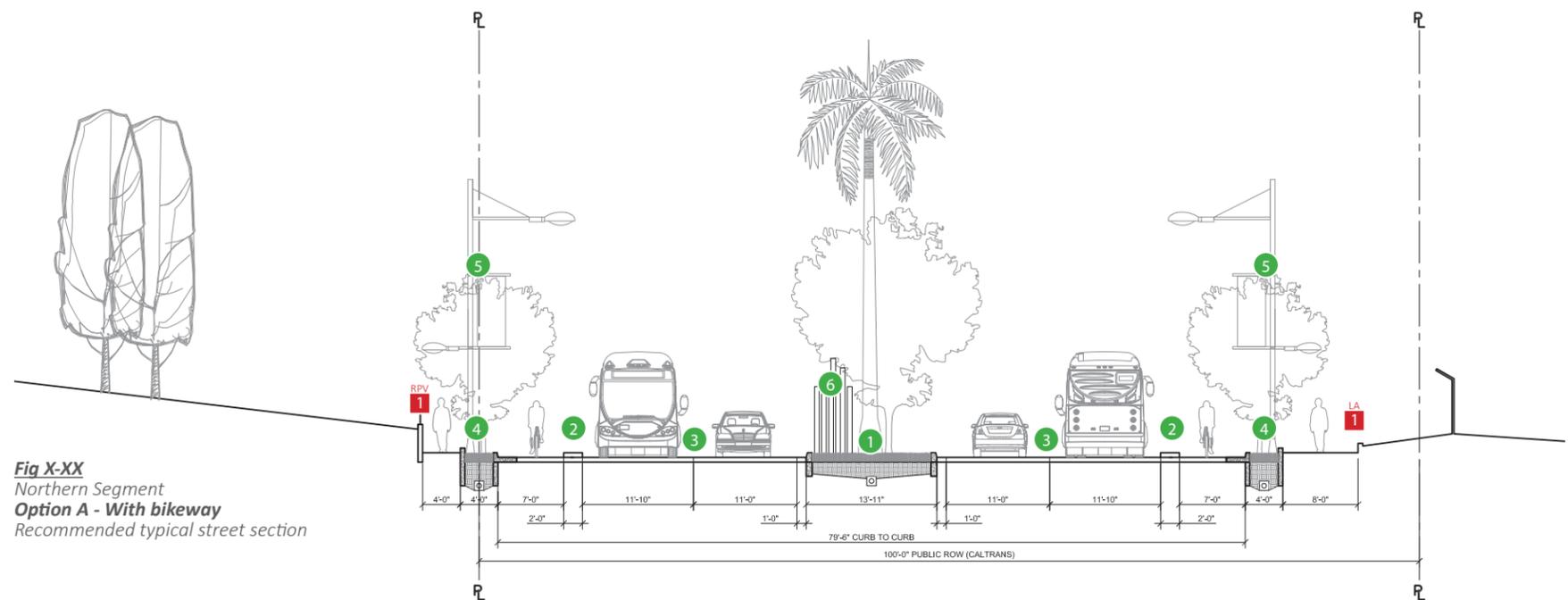
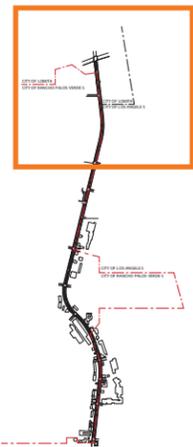


Fig X-XX
Northern Segment
Option A - With bikeway
Recommended typical street section



NORTHERN SEGMENT

OPTION B- No bikeway

The public realm (street and adjacent pedestrian and/or planting zones) includes both Caltrans ROW and adjacent public or private land in either RPV or LA. Complete sections, including multiple jurisdictions, are shown to facilitate consistent implementation along the corridor. Recommendations apply to improvements made in both the cities of RPV and LA, unless otherwise specifically noted.

Approvals: All recommendations are subject to the applicable regulations and approvals of the jurisdiction in which the improvements are located, which varies along the length of the corridor. Recommendations listed under "Public Realm" are also subject to additional regulations and approval by Caltrans.

Public Realm

- 1w Locate street trees within streetscape planting zones and curb extensions, spaced approximately 60ft apart. See Section X-XX.
- 1e Locate street trees within planted curb extensions when they occur. See Section X-XX.

Private Realm

- 1 Not applicable.

Fig X-XX
Northern Segment
Existing typical street plan

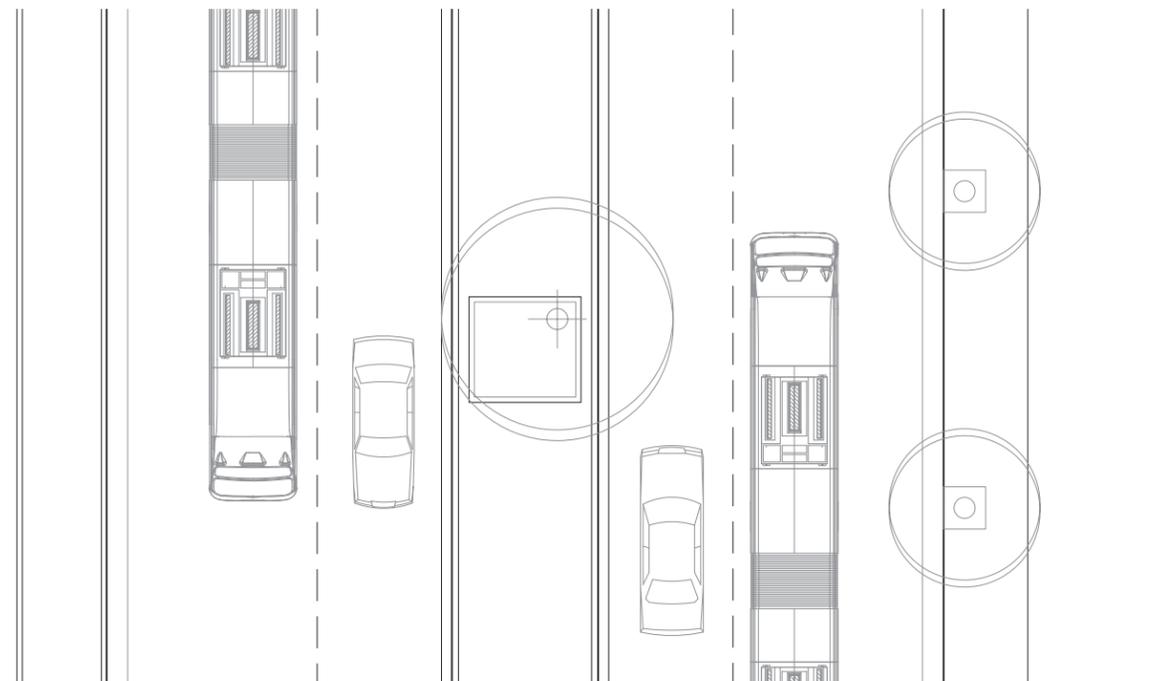
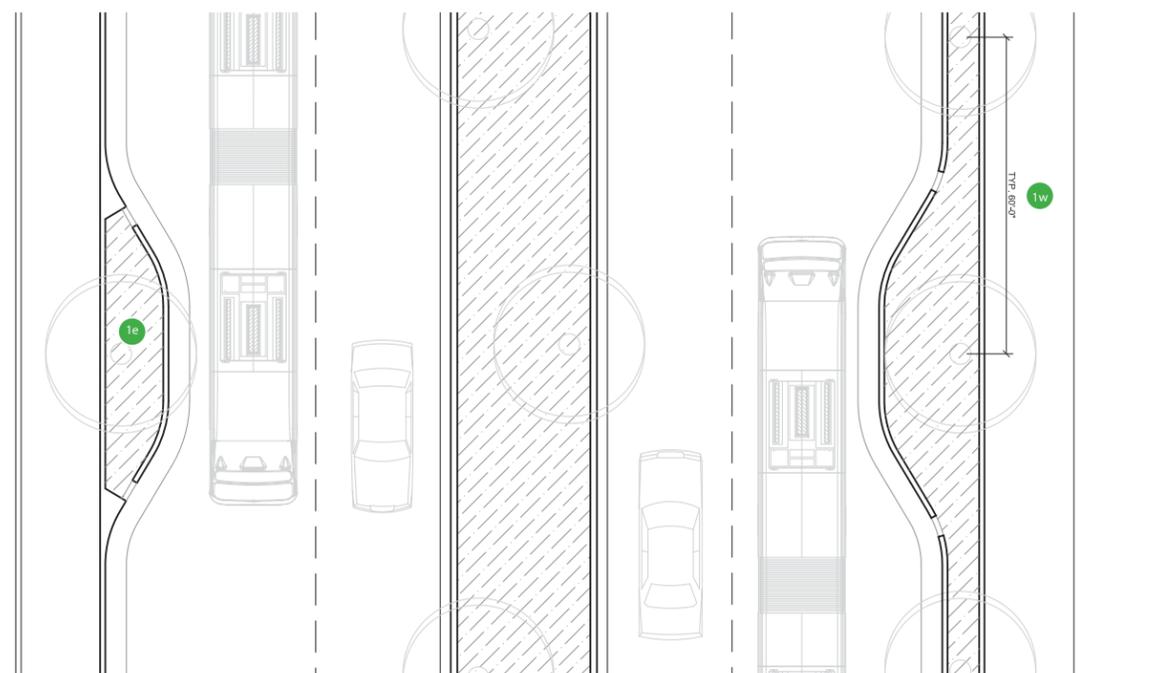


Fig X-XX
Northern Segment
Option B - No bikeway
Recommended typical street plan





Northern Segment

OPTION B- No bikeway

Public Realm

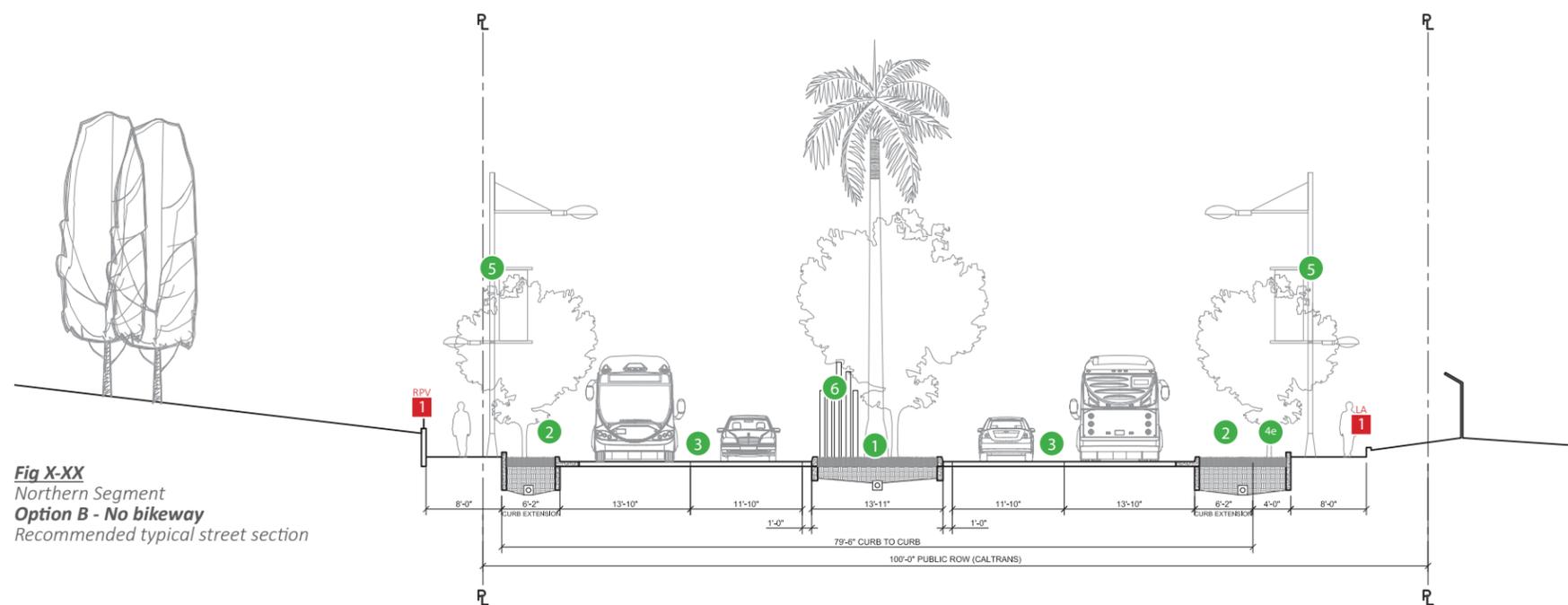
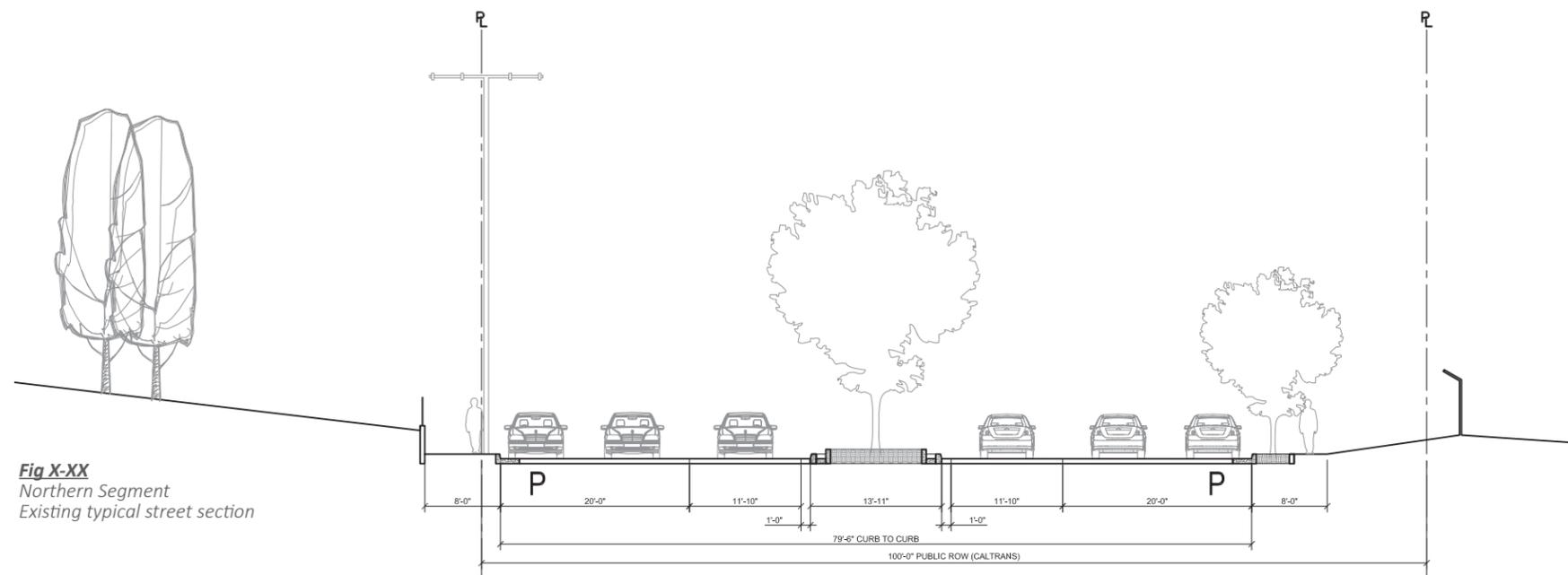
- 1 Same as Option A.
- 2 Remove existing on-street parking and add planted curb extensions spaced approximately every 100ft. See Sec X-XX.
- 3 Same as Option A.
- 4e On east, add new sidewalk planting. Integrate with planted curb extensions when they occur. See Sec X-XX.
- 5 Same as Option A.
- 6 Same as Option A.

FOR CITY OF RANCHO PALOS VERDES (RPV) ONLY

- ^{RPV} 1 Same as Option A.

FOR CITY OF LOS ANGELES (LA) ONLY

- ^{LA} 1 Same as Option A.





3

Guidelines for Private Development

The following standards underscore basic design principles that are intended to produce high-quality buildings, memorable places, and a vibrant urban realm. They are not intended to be indicative of any style, but to encourage innovation and good urban form.

3.1 Building Design and Programming

Ground Floor Uses and Street Level Design

1. Along Western Avenue, 80% of the ground floor should consist of active pedestrian-oriented uses.
 - Pedestrian-oriented uses include active uses that are accessible to the general public, generate walk-in pedestrian clientele, and contribute to a high level of pedestrian activity in the public realm.
 - Typical pedestrian-oriented uses include ground-floor retail, such as retail shops and grocery stores, as well as restaurants, outdoor dining areas, bars, theaters, performing arts, recreation and entertainment, personal and convenience services, building lobbies and building common areas, civic uses, libraries, museums, galleries, and public plazas.
2. Buildings should be designed to “address” Western Avenue by creating/continuing a strong wall that defines the edge of the public ROW.
 - The primary/grade level of buildings should meet the Western Avenue sidewalk, and intersecting streets, at grade.
 - Pedestrian-oriented uses should have a floor elevation that is level with the elevation of the adjacent sidewalk.
3. Entrances to uses on ground and upper floors should open onto a public right-of-way.
4. Primary entrances oriented only towards parking lots are discouraged.
5. Ground-floor floor-to-ceiling height should be at least 15 feet, as measured from the adjacent sidewalk, to accommodate retail uses.

6. Each storefront bay should contain an entrance. The primary entrance to each commercial space on the ground floor should be located on the front façade along the street.
7. Outdoor dining adjacent to the sidewalk is encouraged. It may be provided along segments of the building’s front façade that are set back from the property line, or on the sidewalk.
8. Architectural features such as canopies, awnings, building-mounted lighting, and other design features should be incorporated into the ground floor to add human scale to the pedestrian experience.

Treatment of Building Facades

1. Innovative and imaginative design and architecture is strongly encouraged.
2. Corners and special places should be emphasized



through changes in height and building form.

3. The maximum length of a building should be limited to 200 feet.
 - Where parcels measuring over [200 feet] exist, an open space, paseo, or street should be introduced in order to limit the maximum length of a building to 200 feet.
4. Variation in wall plane, building height, and roof form is strongly encouraged to reduce the scale and bulk of

buildings, and to add visual interest.

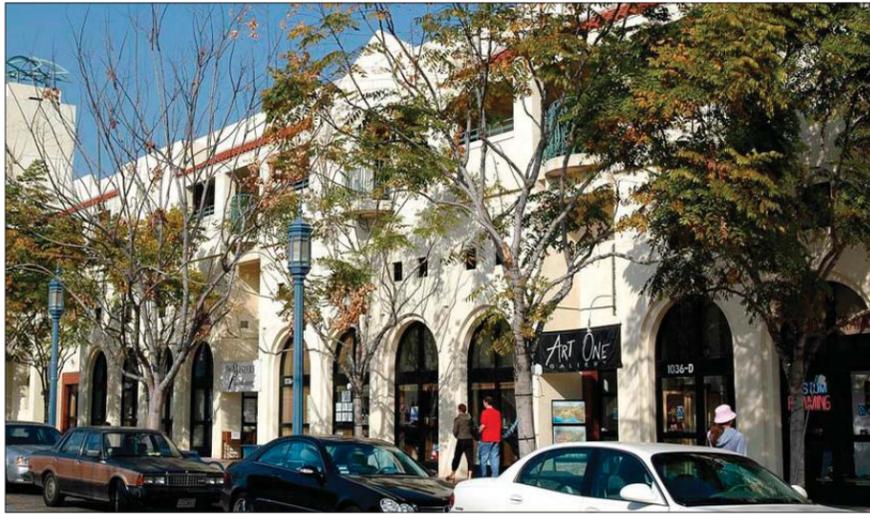
- Variation and expression of building details, form, line, colors, and materials may be used to create visual interest.
 - Individual units should be expressed wherever possible. This may be accomplished in a variety of ways, such as through a change in wall plane, change in color, or change in roof form.
5. Street-facing building facades should incorporate pedestrian-scaled elements such as balconies, awnings, doors, and windows to enliven the street edge, and increase safety by adding “eyes on the street.”
 6. Blank walls, without windows, doors, or other articulation, are strongly discouraged.
 - The maximum length of any blank wall should be limited to 20 feet.

Residential Uses

1. Ground level residential is discouraged along Western Avenue. Ground floor uses should be dedicated to active commercial and retail uses.
2. Where residential is proposed, residential units should face the street with windows, front entry doors, balconies. Rooms such as living rooms and dining rooms shall be oriented fronting toward the street and/or any adjacent private space.
 - Where courtyards, paseos, or greenways are proposed, residential units should face these spaces with windows, front doors, porches, and patios, and according to the standards of this section.
 - Service rooms and areas shall be oriented to the rear of the lot.

Transparency

1. Street facing facades of non-residential uses should be primarily composed of clear, non-reflective glass that allows views of the indoor space.
2. The maximum height of the bottom sill of required display windows should not exceed 30 inches above the adjacent sidewalk. The minimum head height for storefronts and windows at the ground floor should be 80 inches above the adjacent sidewalk.



3.2 Open Space

To promote placemaking and retail activity along the corridor, public open spaces such as greens, parks, plazas and paseos are encouraged. These open spaces can vary in size, form, and character but should all contribute to a well-connected public realm.

Parks, Plazas, and Greens

1. For parcels over 2.5 acres, 10-15 percent of the parcel area should be dedicated to programmed open space, designed as an “organizing element” and central feature of the development.
2. Parks, plazas, paseos, or greens should be located and designed to be accessible and visible from Western Avenue.
 - Where possible, parks and plazas shall be located at intersections or adjacent to mid-block pedestrian crossings and be prominently integrated with the sidewalk and street.
 - Plazas at corners are encouraged to include outdoor dining space for adjacent restaurants.
 - Sidewalks should be provided connecting parks, plazas, paseos, and greens directly to Western Avenue.
3. Where proposed, parks, plazas, or greens should be designed as pedestrian-oriented and public in nature.
 - Public open spaces should include flexible area for public gatherings, such as lawn area or a paved plaza, at a scale that maintains intimacy.
 - Shade trees, pedestrian lighting, seating, seat walls, plazas, fountains, public art, and other high quality design features should be incorporated.
 - Pedestrian lighting shall be incorporated to provide comfort and safety.
 - Public parks and plazas may include an edge element such as a low hedge or seat wall but shall not be fenced or gated.

Pedestrian Paseos

New connections and corridors should be created as larger sites are developed.

1. Where blocks are longer than 200 feet or where a destination, view, or circulation path warrants a mid-block pedestrian connection, publicly accessible streets, open spaces, or paseos shall be provided. [This item needs to be coordinate with related Building Design guidelines, and Mobility guideline.]
2. Pedestrian paseos should be considered open space and include elements such as shade, seating, and water features.
3. Pedestrian lighting shall be incorporated to provide comfort and safety.
4. Paseos should be at least 20 feet wide and include considerations for temporary and emergency vehicle access.

Pedestrian Connectivity [Note: this section may be moved to Mobility]

In addition to creating great urban spaces in Downtown, it is critical to develop a strong pedestrian network that makes traveling between these spaces easy, safe, and enjoyable.

1. Disruption of the existing street grid is prohibited; however, new streets, alleys, or pedestrian connections may be added.
2. The pedestrian network shall include a great pedestrian zone (discussed in Section X-X), legible and well-located crosswalks, mid-block pedestrian connections, and wayfinding elements such as street signs and kiosks.
3. The incorporation of retail and residential along pedestrian zones is highly encouraged. Additionally for safety and “eyes on the street” all buildings addressing pedestrian zones, open space, parks, plazas, and /or paseos, shall incorporate active uses, building entries, or other active facades to address the pedestrian zone and add interest to the public realm.
4. Pedestrian and bicycle priority zones may be incorporated into pedestrian networks.

Landscaping

1. Landscape elements and gardens should be used to define building entries, pathways, and semi-private open spaces, and to add special character to building setbacks.
 - Integrate roof-top components such as landscapes gardens, trellises, and sustainability features.
2. Drought tolerant, native plant materials should be used wherever possible.
3. Landscape plans should incorporate provisions for stormwater runoff, including bio-swales, or other comparable LID methods.



Architecture, textured walkways, landscape features, and seating areas create enjoyable open spaces and can make retail streets a civic destination.



Programmed open space should be a defining “organizing element” of parcels over 2.5 acres, and connect directly to Western Avenue.



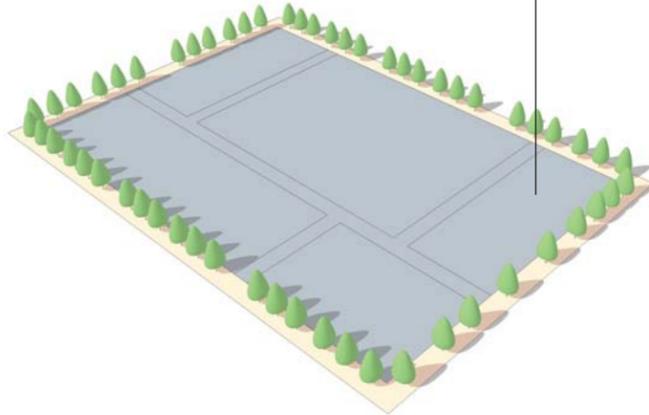
[Discuss of role of roof terraces/gardens in caption and add to text.]

3.3 Access and Parking

Parcel Access

1. Parcels under 2.5 acres in size should include no more than 1 curb cut along Western Avenue.
 - As feasible, two or more adjacent parcels should share access to limit the frequency of curb cuts along Western Avenue.
2. For parcels over 2.5 acres, a street and block pattern should be established in order to create a connected, pedestrian-scaled block and street pattern.
 - Any new street should look and feel like a well designed urban street that supports a high level of pedestrian activity.

For parcels over 2.5 acres, a street and block pattern should be established.



Parking

1. Parking structures are discouraged along Western Avenue, and other frontages with active, pedestrian-oriented uses.
 - Parking lots should be placed at the rear of the parcel and should not prevent proposed buildings from having a direct relationship with Western Avenue.
 - Access should be taken via the alleys serving the site or, on corner lots, at the street frontage that does not contain active ground-floor uses.

- Access to parking lots or structures should be located along side streets wherever possible, and coordinated among multiple parcel owners.
2. Parking lots should include well-positioned, shaded sidewalks to facilitate pedestrian-orientation, walkability, and connectivity between Western Avenue and multiple uses.
 3. Parking lots should include shade elements such as trees, vine-covered trellises, or overhead solar panels. The design of shade elements should consider safety and visibility.

3.4 Service

Service and Loading

1. Loading, service areas, storage, and trash collection areas shall be located away from Western Avenue and other primary pedestrian routes.
 - Loading, service areas, storage, and trash collection areas should be located at the rear of buildings, or in a coordinated location that is screened from view by the use of walls, high-quality fencing, planting, or a combination of these solutions.
 - Landscaping and walls should be treated in a manner that is consistent with the architectural style of the building.



The Southern Segment's street edges are dominated by surface parking lots (in gray) while buildings (in orange) are pushed to the backs of parcels. In order to bring retail activity to the street edge, new development projects should locate parking lots at the rear of the parcel.



New streets should look and feel like a well designed, active, and pedestrian-oriented street.

3.5 Lighting and Signage

Lighting

1. Road, pedestrian, and feature lighting should utilize a coordinated palette, or “family” of fixtures, to create a unified character along the length of the corridor.
 - Street lighting may utilize either a single or double head fixture, and optional banners. The selected style should be implemented consistently along the length of the corridor.
 - All lighting fixtures shall be consistent in color, with a powder cast pole.
 - Pedestrian-scaled poles, building-mounted lighting, or bollards should be utilized for mid-block connections, and at parks, plazas, and greens.
 - Light fixtures shall minimize light spillage with full cut off luminaries.
2. [See section X-X for additional lighting guidelines. This section may be moved to consolidate two sections into one.]

Signage

1. A coordinated signage program should be developed.
 - Signage shall be submitted to X, y, z and subject to design review. [Should we clarify the any differences between communities?]
 - Visual clutter shall be minimized by attaching street signage to poles when possible. When a separate pole is used, the pole shall be colored and powder coated to match the style of the lighting fixtures.



4

Guidelines for the Public Right-of-Way

4.1 STREETScape AND STREET FURNITURE

Streetscape improvements include widened sidewalks with continuous landscaping and trees, the addition of street furniture, such as seating, planters, newspaper racks, and trash receptacles, as well as new street and pedestrian lighting, and the under-grounding of utilities.

1. A streetscape program should be developed to support the consistent implementation of the Guidelines within both the Cities of LA and RPV.
 - The streetscape program shall be submitted to [x,y, and z] and subject to design review.
2. When selecting street furniture, such as benches, trash receptacles, and bicycle racks, a “family” or “kit of parts” approach should be utilized to promote a consistent design theme, character, and finish.
 - The “family” shown on page X-X is identified for illustrative purposes only, and is not intended to be indicative of a brand selection.
 - A “family” of fixtures should be selected as part of the streetscape planning process.
 - As part of the selection process, sustainability benefits of the product should be considered wherever possible.
3. Custom furniture such as stone benches, bicycle racks, etc. may be permitted subject to design review.

Sidewalks and Paving

1. Public walkway connections between streets and buildings are required.
 - Front doors and entrances shall be directly accessed from the sidewalk.
2. For guidelines related to crosswalks and other mobility improvements, see [Section X-X].
3. For guidelines related to landscaping, see [Section X-X].

Seating

1. A minimum of [specify dimension] clear should be

provided around the seating.

2. All seating should be selected to coordinate with the Western Avenue “family” of street furniture, with a design similar to [insert] and of a consistent color, with a [specify finish].

Newspaper Racks

1. A minimum of 18” clear should be provided around the newspaper rack.
2. All newspaper racks should be selected to coordinate with the Western Avenue “family” of street furniture, with a design similar to [insert] and of a consistent color, with a [specify finish].

Trash Receptacles

Properly distributed trash receptacles will help maintain an orderly street environment. Trash receptacles should be located in proximity to other pedestrian amenities such as bus shelters and seating.

1. A minimum of 18” clear should be provided around the trash receptacle.
2. All trash receptacles should be selected to coordinate with the Western Avenue “family” of street furniture, with a design similar to [insert] and of a consistent color, with a [specify finish].

Lighting

There are two types of lighting proposed for the project area: roadway lights (“street lights”) and pedestrian-scale lights (“pedestrian lights”). Street lights provide illumination of both the roadways and sidewalks to the required levels. Pedestrian lights supplement the street lights, contribute to the pedestrian scale of the street, and create an environment that feels safe and secure for pedestrians and cyclists.

Siting

1. New street and pedestrian lighting should be integrated along the length of the Western Avenue right-of-way.
2. Locations such as ramps, crosswalks, transit stops, and seating areas that are used at night should be visible and lit.

3. Pedestrian lighting should be provided to accentuate focal points such as parks, plazas, greens, paseos, and other pedestrian linkages, such as sidewalks connecting parking areas to commercial, in order to encourage evening and/or night time use.

Design

1. All street and pedestrian lighting should be coordinated to create a cohesive streetscape theme, contribute to the branding of Western Avenue, and be compatible with the design, materials, scale, and character of other improvements described in the Guidelines.
2. All lighting shall be a consistent color, with a powder cast pole.
3. Light fixtures should minimize light spillage with full cut-off luminaires.
4. Visual clutter shall be minimized by attaching street signage to poles when possible. When a separate pole is used, the pole shall be colored and powder coated to match the style of the selected lighting fixtures.
5. Clamp-on brackets for banners and/or hanging planters should be considered as part of the streetscape program.
6. As fixtures are upgraded, sustainability features, such as LED, timers, and dimmers, should be considered wherever possible.

Utilities and Equipment

1. Existing overhead utilities should be relocated below grade. The relocation of utilities should be coordinated with the sequencing of construction activities to avoid conflict with planned streetscape improvements.
2. New utility poles, transformers, back flow preventers and other utilities should be placed in the least obtrusive location.
3. New utility lines should be placed underground to reduce impacts to historic character of preservation zone.
4. Mechanical and electrical equipment shall not be placed in such a manner so as to create ambient noise and/or environmental pollution on adjacent residential properties.



» LUMINAIRE > DOS-LR
BRACKET > DBC-1A
POLE > DAR3



Streetscape components may vary from neighborhood to neighborhood, but should generally include a coordinated approach to items such as seating, bicycle racks, and trash receptacles.



Seating and street furniture may vary to create interest throughout the community or incorporate public art, but should generally adhere to a coordinated palette or family such as the one illustrated within this selection.

The family of fixtures (above and at right) illustrate a coordinate family of street and pedestrian lighting family. The family can be customized to a color, coating, and banner branding, to coordinate with the design of the corridor.

[GENERAL NOTE: This is one example of a “family” of lighting and street furniture. This can be adjusted after discussion to meet other stylistic goals, i.e. more modern, different color, etc.]

4.2 MOBILITY AND COMPLETE STREETS IMPROVEMENTS

The following mobility guidelines are intended to help evolve Western Avenue from a predominately auto-oriented corridor into a “complete street” where the needs of all users (pedestrians, bicyclists, transit users, and automobiles) are equally met. The State of California has emphasized the need for complete streets by enacting the Complete Streets Act of 2008 (AB 1358) and Deputy Directive 64-R1. In cooperation with Caltrans, a mobility program should be developed to support the consistent implementation of the Guidelines within both the Cites of Rancho Palos Verdes and Los Angeles.

The following resources were referenced to develop these guidelines:

- City of Los Angeles. Complete Streets Manual, Draft. February 2014.
- Caltrans. Highway Design Manual. March 2014.
- Caltrans. Main Street, California. 3rd Edition. November 2014.

Travel Lanes and Speed

Reduced-scale and reduced-speed environments improve safety, accessibility, and comfort of all users. The following recommendations should be considered in conjunction with the proposed streetscape improvements:

1. Consider the reduction of the posted traffic speed to a maximum of 40 mph in the Northern Segment and a maximum of 35 mph in the Middle and Southern Segments to facilitate safe driving, walkability, and biking.
2. Consider the reduction of the width of travel lanes to accommodate new roadway elements (e.g. medians, bikeways, expanded sidewalks, curb extensions, etc). The following travel lane dimensions do not include the width of required shoulders, curbs, or gutters (See Caltrans Highway Design Manual).
 - Minimum width of travel lane: 10'-6"
 - Minimum width of travel lane with high bus/truck traffic: 11'-0"
 - Minimum width of left-turn lane: 11'-0"

Medians

Raised medians can reduce traffic conflicts between pedestrians, bicyclists, and vehicles, thereby improving safety and improving traffic flow. Landscaped medians help improve the aesthetic character of the street and support environmental benefits like stormwater management.

1. Establish a continuous raised landscaped center median along the entire corridor, with necessary interruptions for left-turn pockets, pedestrian refuge islands, and mid-block crossings. See X-XX for median planting.
2. The minimum width of the median should be 12'-0", with the exception of left-turn pockets, including curbs.
3. At all left-turn pockets, a planted median nose of a minimum width of 4'-0", including curbs, is encouraged. If there is insufficient space, a pavement-marked median nose will suffice.
4. Where mid-block crossings are provided, a pedestrian refuge island should be incorporated within the median.
5. At all intersections, provide a pedestrian refuge island within the median if the median is wider than 6'-0", including curbs (See Caltrans Highway Design Manual).

Curb Extensions / Bulb-Outs

Curb extensions (also known as bulb-outs) are a traffic calming technique that expands the sidewalk into the roadway. Curb extensions improve visibility between pedestrians and motorists, shorten the distance pedestrians must cross, slow turning vehicles, and provide additional space for street furniture and landscape.

1. Provide curb extensions at all mid-block crossings and intersection crossings.
2. Curb extensions should expand the width of adjacent on-street parking lanes at a minimum width of 8'-0", without encroaching into adjacent bikeways.
3. At mid-block crossings, curb extensions should be a minimum of 15'-0" in length.
4. At intersections, curb extensions should turn the corner at a 20'-0" radius.

5. When space allows, provide streetscape planting and furniture at all curb extensions. See Sec X-XX for guidelines on streetscape planting and Sec X-XX on streetscape furniture.

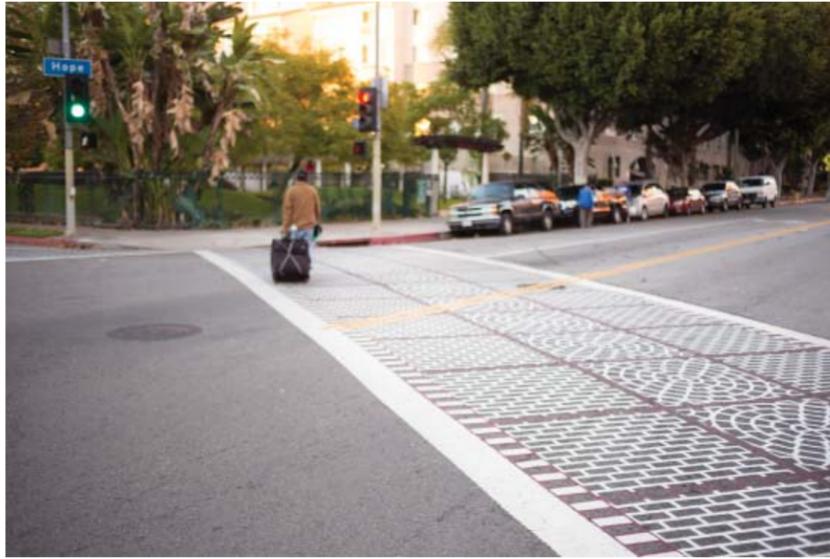
Pedestrian Crossings

Pedestrian crossings should provide the most direct, shortest, easily accessible, and visible path of travel to be able to safely and comfortably cross the street.

1. Provide marked crosswalks at all intersections and mid-block crossings.
2. All crosswalk markings should be “high-visibility,” e.g. Continental, Ladder, Diagonal (See California Manual on Uniform Traffic Control Devices).
3. The minimum width of crosswalks should be 15'-0".
4. Provide curb ramps at all crossings. All curb ramps must be compliant with ADA standards and all other current local, state, and federal regulations.
5. When crossings occur at curb extensions, provide directional curb ramps (i.e. curb ramps that are aligned in the direction of pedestrian crossing).
6. Provide mid-block crossings when block lengths exceed 500'-0" [Confirm: should this be 400'?] and at “special places” (i.e. locations in which there is a significant pedestrian desire path to/from pedestrian destinations, like building entrances, plazas, parks, paseos, etc).

Sidewalks

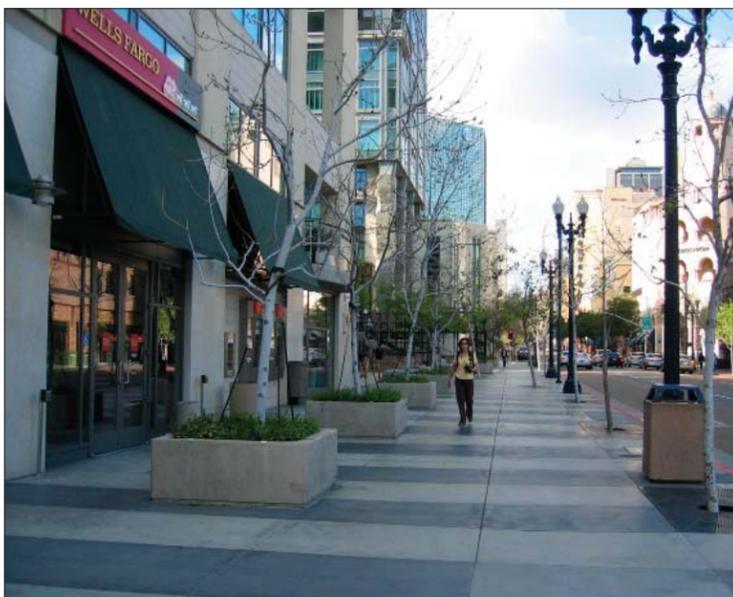
See [Section X-XX] for guidelines related to sidewalks.



Pedestrian crosswalks.
[Explanation and confirmation of photos pending confirmation of sections.]



[Confirm photos and insert description here.]



Pervious paving and LID solutions can be utilized along sidewalks, parking lanes, parkways, and/or bike lanes. More than just contributing to sustainability, they can aid in placemaking, slow traffic, and contribute to the pedestrian-scale of a community.

Bikeways

As of the date of this document, there is no official bikeway designation along the corridor. In the long-range bicycle plans, for both the City of Rancho Palos Verdes and the City of Los Angeles, a bikeway is recommended along Western Avenue, within the project area. Any future plans should consider a balanced roadway approach to facilitate the appropriate safety, accessibility, and comfort of all users.

See [Section X-XX] for bikeway guidelines respective to each Segment. For further information, also refer to the Caltrans Highway Design Manual and California Manual on Uniform Traffic Control Devices.

[*Note: This section need to be discussed and confirmed once the approach to bike lanes is finalized.]

Class II Bikeway/Bike Lane

Caltrans defines a Class II Bikeway as a designated right of way within the roadway, typically demarcated by pavement markings and signage.

1. The minimum width of a Class II bikeway should be 5'-0".
2. When adjacent to a curbside parking lane, an additional 2'-0" buffer zone should be incorporated between the bike lane and the parking lane, to minimize potential bicyclist collisions with car doors.
 - Bike lanes should be painted so they are clearly recognizable.
 - Buffer zones should be clearly demarcated with pavement markings.

Class III Bikeway/Bike Route

Caltrans defines a Class III Bikeway as a shared right of way with vehicles on the roadway (i.e. "sharrow"), typically demarcated by pavement markings and signage.

1. The minimum width of a Class III bikeway is represented by the minimum standards for travel lanes and shoulders.

Cycle Track

As of the date of this document, Caltrans has not yet identified a Cycle Track as an officially designated Bikeway. A Cycle Track

is a curbside, exclusive, separated, and protected right of way for bicyclists.

1. The minimum width of a Cycle Track should be 5'-0".
2. Cycle Tracks can be at-grade with the roadway, level with the adjacent sidewalk, or set at an intermediate level between the roadway and the sidewalk.
3. Cycle Tracks should be protected and separated from the roadway by a minimum width of 2'-0" using one or more of the following methods:
 - Raised curb buffer
 - Pavement marking buffer
 - Planting buffer (i.e. planter boxes)
 - Bollards or other protective barrier

Bicycle Parking

1. Bicycle racks and lockers should be focused in close proximity to bus shelters and public amenities such as open spaces, parks, and greens.
2. Bicycle racks and lockers should be placed in a safe, convenient and visible locations, easily visible from areas such as building entrances, security offices, lobbies, public areas, and walkways.
 - Bicycle parking areas should be adequately lit.
 - Bicycle parking areas should not obstruct pedestrian or vehicular traffic flow, and should be placed where riders can safely and easily dismount, and walk to building entrances.

Signal Synchronization

1. Coordinated signal synchronization facilitates efficient traffic flow and improved safety for all modes.
 - A signal synchronization program should be developed between Caltrans and the Cities of Rancho Palos Verdes and Los Angeles for a synced approach to traffic (inclusive of vehicle, pedestrian, and bicycle) signalization along the entire corridor.



Class II Bike Lane



Class III Bike Lane

[GENERAL NOTE: This page is shown for discussion purposes only, and will be updated pending confirmation of preferred street section.]



A variety of Cycle Tracks are shown, above and at left, including single-way or multi-way.

4.3 LANDSCAPE DESIGN

The suggested landscape design approach, including the street tree and plant palette, has been designed to address the varying nature of each of the three segments of the corridor, the north segment, the middle segment, and the south segment. The design should respond and contribute to the experience and character of each segment, including the degree of retail and pedestrian traffic.

1. Regardless of location, the vegetation in each segment should be California friendly, drought tolerant, water-wise, and low-maintenance.
2. The Northern Segment of the corridor is primarily auto-oriented.
 - Plantings in the median within the streetscape planting zones should be scaled to the needs of an automobile traveling, for example, tall grasses that bend and blow as cars drive by.
 - The Northern Segment serves as a gateway to the corridor and should announce a sense of arrival by utilizing “special” plants and trees – grander, colorful, visible clustering, etc.
 - The Northern Segment, unlike the rest of the corridor, is also unable to develop a street wall via new development. Tall, vertical trees (palms, pines, etc.) should be introduced, to provide the sense of a “street wall,” through continuity and verticality.
3. The median in the Southern and Middle Segments should build on the existing pines and palms, to retain and reinforce their verticality. Grasses should be introduced at the ground plane.
4. In the Southern Segment of the corridor, trees should complement and facilitate the retail experience.
 - Trees should provide shade to pedestrians and reinforce the pedestrian-scale of the streetscape.
 - Street trees should not be overly tall, nor block views to stores and signs.
 - Trees should be selected so that, when mature, canopies should start at 15 feet above ground, and permit visibility to storefronts and signage, without “limbing up” or excessive pruning or shaping.

Streetscape Planting

1. Streetscape design should complement adjacent land use needs.
 - In the Southern Segment, streetscape planting zones should be designed in concert with sidewalk cafes and retail uses.
 - Where adjacent to on-street parking, streetscape planting zones should be non-contiguous to permit pedestrian access to parked vehicles.
2. [Discuss potential conflicts with bus stops. See Section X-X.]
3. [Discuss potential conflicts with underground and above ground utilities. See Section X-X.]
4. Street furniture, right of way signage, and bulb-outs or special design areas, should be carefully coordinated with the landscape plan.
5. Streetscape planting zones should be designed to accommodate adequate soil volume to support selected plant material. See plant palette [Section X-X].
 - Individual tree wells are discouraged.
 - Parkways should be designed to accommodate two or more street trees where possible, and be a minimum size of [x ft wide by 6 ft in length].
 - The use of tree grates is discouraged, in favor of decorative seat walls, edging with pavers, cobbles, and/or well placed street furniture and seating. [Clarify]
6. Streetscape planting zones should be linked by a common comprehensive drainage system, in concert with existing storm drainage and potential water sensitive urban design measures.
7. All parkways should be irrigated per plant palette recommendations.
8. [Note: Additional guideline(s) may be added here once street cross-sections and approach are finalized.]

Median Planting

1. Hardscape maintenance strips should be incorporated into the design of all medians, per Caltrans requirements.
2. Special care shall be exercised in the selection of plant material in areas where the median measures 3 feet or less in width, as measured from back of curb.

3. [Note: Additional guideline(s) may be added here once street cross-sections and approach are finalized.]

Low-Impact Development (LID) Design Options

1. The use of bio-swales, appropriately located curb breaks, roof gutter diversions, permeable parkway, median, and bike-lane surfaces, and other LID design options are all encouraged.
2. LID solutions for sidewalks, plazas, and special design areas, are encouraged.
3. Where utilized, the design of bio-swales should be carefully coordinated with the urban design of Western Avenue.
4. [Note: Additional guideline(s) may be added here once street cross-sections and approach are finalized. They may include plant material selection, intermittent rainfalls/ summer irrigation, sedimentation / maintenance, appearance, cost.]



In the Northern Segment, tall grasses that bend and blow as cars drive by, can be used to define this part of the corridor.



The design of medians, in the Middle and Southern Segments, should incorporate existing mature trees.



In the Southern Segment, parkways should be designed in concert with sidewalk cafes and retail uses.



The use of bio-swales, appropriately located curb breaks, roof gutter diversions, permeable parkway, median, and bike-lane surfaces, and other LID design options are all encouraged.

4.4 STREET TREE AND PLANT PALETTE

[Add an overview here to describe what a palette is and how it should be used.]

Planting Selection

1. Selection of water-wise plant material is strongly encouraged.
2. Plant material should be selected with the following characteristics:
 - Tolerance of urban conditions
 - Ultimate size and form
 - Low litter production
 - Ease of maintenance
 - Multi-season interest
 - Avoidance of prohibited or invasive species
 - Avoidance of thorns and spikes adjacent to high pedestrian areas.

[Image to be inserted.]

3. Planting material, including trees, should be selected for desired characteristics, avoiding the need for frequent pruning, shearing, or shaping.
4. Plant material groupings should balance dormancy periods of included species.

5. Planting should be grouped by water use hydro-zones to maximize efficiency of water use.
6. All plant material should be irrigated per water-wise planting recommendations, and provided adequate drainage.



7. Where needed, drainage areas should be connected to existing stormwater conveyance or option LID design solutions.
8. Where palms are used as street trees, it is strongly recommended to alternate with lower broad-leaf ornamental deciduous or evergreen trees.
9. Triangularly spaced double tree rows are encouraged at special design areas, defining pedestrian entries, nodes, or crossings, space permitting.
10. [May need to clarify deciduous vs. evergreen]

Coordination of Planting Approach along Corridor

1. The street tree and planting approach should be coordinated, by segment (i.e. North, Middle, and South), in order to ensure the consistent implementation of the tree and planting design guidelines.
2. Existing mature trees shall be maintained wherever possible, and incorporated within the design of the corridor.
3. Refer to the Planting Palette, provided on the following

pages, for additional direction related to the selection of planting material, location, and sizing.

4. [Note: Additional guideline(s) may be added here once street cross-sections and approach are finalized.]

Site Specific Requirements

1. Within the City of RPV, [describe], the following requirements shall apply.
2. Within the City of Los Angeles, [describe], the following requirements shall apply.
3. A maintenance agreement, defining responsibilities, maintenance and pruning procedures, should be negotiated with Caltrans, RPV, and LA to ensure the continued aesthetic quality of the corridor.



Street Tree and Plant Palette: Southern Segment



KOELREUTERIA PANICULATA



TIPUANA TIPU



PINUS PINEA



PHOENIX DACTYLIFERIA



WASHINGTONIA ROBUSTA

SHRUBS & GROUNDCOVER:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
AGAVE ANGUSTIFOLIA VAR. MARGINATA	CARIBBEAN AGAVE	3'-5'	3'-5'	5 GALLON
ALOE STRIATA	CORAL ALOE	2'	2'	5 GALLON
BOUGAINVILLEA 'LA JOLLA'	BOUGAINVILLEA 'LA JOLLA'	4'-5'	4'-5'	5 GALLON
CAREX DIVULSA	GREY SEDGE	1'-2'	1'-2'	5 GALLON
DIANELLA REVOLUTA 'LITTLE REV'	BABY BLISS FLAX LILY	2'-3'	1'-2'	5 GALLON
DIETES GRANDIFLORA	FORTNIGHT LILY	3'	3'	5 GALLON
DIETES VEGETA	AFRICAN LILY	3'	3'	5 GALLON
JUNCUS EFFUSUS	COMMON RUSH	3'	1'-2'	1 GALLON
JUNCUS PATTENS	COMMON RUSH	3'	3'	5 GALLON
KNIPHOFIA 'ECHO ROJO'	ECHO ROJO RED HOT POKER	3'-4'	3'-4'	5 GALLON
LANTANA MONTIVIDENSIS	PURPLE TRAILING LANTANA	2'	10'	5 GALLON
LEUCOPHYLLUM FRUTESCENS	TEXAS RANGER	3'-4'	3'-4'	5 GALLON
LEYMUS CONDESATUS 'CANYON PRINCE'	CANYON PRINCE WILD RYE	3'-4'	3'-4'	5 GALLON
MUHLENBERGIA RIGENS	DEER GRASS	1'	2'-3'	1 GALLON
PHORMIUM TENAX	NEW ZEALAND FLAX	5'-7'	3'-5'	5 GALLON
SENECIO TALINOIDES VAR MANDRALISCAE	BLUE CHALK STICKS	1'	2'-3'	1 GALLON
SOLIDAGO CALIFORNICA	CALIFORNIA GOLDENROD	1'-2'	2'-3'	1 GALLON
TULBAGHIA VIOLACEA	SOCIETY GARLIC	1'-2'	2'-3'	1 GALLON

TREES:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
KOELREUTERIA PANICULATA	GOLDEN RAIN TREE	30'-40'	30'-40'	48" BOX
TIPUANA TIPU	TIPU TREE	40'-50'	25'-40'	48" BOX
PHOENIX DACTYLIFERIA	DATE PALM	40'-50'	-	18'-25' BTH
PINUS PINEA	ITALIAN STONE PINE	30'-40'	30'-40'	48" BOX
WASHINGTONIA ROBUSTA	MEXICAN FAN PALM	50'-90'	-	18'-25' BTH

PLANTING PALETTE FOR AREAS WITH WATER INDUNATION: SOUTHERN SEGMENT



CAREX DIVULSA



SOLIDAGO CALIFORNICA



JUNCUS EFFUSUS



JUNCUS PATTENS



MUHLENBERGIA RIGENS



TULBAGHIA VIOLACEA

PLANTING PALETTE FOR AREAS WITHOUT WATER INDUNATION: SOUTHERN SEGMENT



ALOE STRIATA



BOUGAINVILLEA 'LA JOLLA'



DIANELLA REVOLUTA



DIETES VEGETA



AGAVE ANGUSTIFOLIA VAR. MARGINATA



ALOE STRIATA



PHORMIUM TENAX



LANTANA MONTIVIDENSIS



LEUCOPHYLLUM FRUTESCENS



MUHLENBERGIA RIGENS



SENECIO TALINOIDES VAR. MANDRALISCAE



LEYMUS CONDESATUS 'CANYON PRINCE'



KNIPHOFIA 'ECHO ROJO'

*Note: A planting palette is provided for each segment, for areas with water inundation. Planting in these areas are expected to be submerged, or partially submerged, during stormwater events, in areas that will be used for stormwater conveyance, and as part of LID solutions.

Street Tree and Plant Palette: Middle Segment



KOELREUTERIA PANICULATA



LAGERSTROEMIA INDICA



TIPUANA TIPU



PHOENIX DACTYLIFERIA



WASHINGTONIA ROBUSTA

PLANTING PALETTE FOR AREAS WITH WATER INDUNATION: MIDDLE SEGMENT



CAREX DIVULSA



SOLIDAGO CALIFORNICA



JUNCUS PATTENS



MISCANTHUS SINENSIS ADAGIO



MUHLENBERGIA RIGENS



TULBAGHIA VIOLACEA

PLANTING PALETTE FOR AREAS WITHOUT WATER INDUNATION: MIDDLE SEGMENT



ALOE STRIATA



BOUGAINVILLEA 'LA JOLLA'



DIANELLA REVOLUTA



DIETES VEGETA



AGAVE ANGUSTIFOLIA VAR. MARGINATA



ALOE STRIATA



KNIPHOFIA 'ECHO ROJO'



HELICOTRICHON SEMPERVIRENS



JUNCUS PATTENS



LEUCOPHYLLUM FRUTESCENS



FICUS PUMILA



MUHLENBERGIA RIGENS



LEYMUS CONDESATUS 'CANYON PRINCE'



SENECIO TALINOIDES VAR. MANDRALISCAE

SHRUBS & GROUNDCOVER:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
AGAVE ANGUSTIFOLIA VAR. MARGINATA	CARIBBEAN AGAVE	3'-5'	3'-5'	5 GALLON
ALOE STRIATA	CORAL ALOE	2'	2'	5 GALLON
BOUGAINVILLEA 'LA JOLLA'	BOUGAINVILLEA 'LA JOLLA'	4'-5'	4'-5'	5 GALLON
CAREX DIVULSA	GREY SEDGE	1'-2'	1'-2'	5 GALLON
DIANELLA REVOLUTA 'LITTLE REV'	BABY BLISS FLAX LILLY	2'-3'	1'-2'	5 GALLON
DIETES VEGETA	AFRICAN IRIS	2'-3'	3'-4'	5 GALLON
FICUS PUMILA	CREEPING FIG	3'-4'	25'-30'	5 GALLON
HELICOTRICHON SEMPERVIRENS	BLUE OAT GRASS	1'-2'	1'-3'	5 GALLON
JUNCUS PATTENS	COMMON RUSH	3'	3'	5 GALLON
KNIPHOFIA 'ECHO ROJO'	ECHO ROJO RED HOT POKER	3'-4'	3'-4'	5 GALLON
LEUCOPHYLLUM FRUTESCENS	TEXAS RANGER	3'-4'	3'-4'	5 GALLON
LEYMUS CONDENSATUS 'CANYON PRINCE'	CANYON PRINCE WILD RYE	3'-4'	3'-4'	5 GALLON
MISCANTHUS SINENSIS ADAGIO	DWARF MAIDEN GRASS	12"	3'-4'	1 GALLON
MUHLENBERGIA RIGENS	DEER GRASS	12"	3'-4'	1 GALLON
SENECIO TALINOIDES VAR. MANDRALISCAE	BLUE CHALK STICKS	12"	3'-4'	1 GALLON
SOLIDAGO CALIFORNICA	CALIFORNIA GOLDENROD	1'-2'	2'-3'	1 GALLON
TULBAGHIA VIOLACEA	SOCIETY GARLIC	1'-2'	2'-3'	1 GALLON

TREES:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
KOELREUTERIA PANICULATA	GOLDEN RAIN TREE	30'-40'	30'-40'	48" BOX
LAGERSTROEMIA INDICA	CREPE MYRTLE	20'-30'	15'-25'	36" BOX
TIPUANA TIPU	TIPU TREE	40'-50'	35'-40'	48" BOX
PHOENIX DACTYLIFERIA	DATE PALM	40'-50'	-	18'-25' BTH
WASHINGTONIA ROBUSTA	MEXICAN FAN PALM	50'-90'	-	18'-25' BTH

*Note: A planting palette is provided for each segment, for areas with water inundation. Planting in these areas are expected to be submerged, or partially submerged, during stormwater events, in areas that will be used for stormwater conveyance, and as part of LID solutions.

Street Tree and Plant Palette: Northern Segment



KOELREUTERIA PANICULATA



PARKINSONIA ACULEATA 'DESERT MUSEUM'



TIPUANA TIPU



PHOENIX DACTYLIFERIA



WASHINGTONIA ROBUSTA

SHRUBS & GROUNDCOVER:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
AGAVE AMERICANA 'VARIEGATA'	CENTURY PLANT	3'-5'	3'-5'	5 GALLON
AGAVE ANGUSTIFOLIA VAR. MARGINATA	CARIBBEAN AGAVE	3'-5'	3'-5'	5 GALLON
AGAVE ATTENUATA	FOX TAIL AGAVE	3'-5'	3'-5'	5 GALLON
ALOE STRIATA	CORAL ALOE	2'	2'	5 GALLON
CAREX DIVULSA	GREY SEDGE	1'-2'	1'-2'	5 GALLON
CEANOTHUS 'JOYCE COULTER'	CREEPING MOUNTAIN LILAC	6'-8'	6'-10'	5 GALLON
DALEA CAPITATA 'SIERRA GOLD'	SIERRA GOLD DALEA	3'-4'	3'-4'	5 GALLON
DIETES GRANDIFLORA	FORTNIGHT LILY	2'-3'	2'-3'	5 GALLON
DRACAENA DRACO	DRAGON TREE	4'-8'	3'-4'	15 GALLON
HELICOTRICHON SEMPERVIRENS	BLUE OAT GRASS	1'-2'	2'-3'	1 GALLON
JUNCUS PATTENS	COMMON RUSH	3'	3'	5 GALLON
KNIPHOFIA 'ECHO ROJO'	ECHO ROJO RED HOT POKER	3'-4'	3'-4'	5 GALLON
LEUCOPHYLLUM FRUTESCENS	TEXAS RANGER	3'-4'	3'-4'	5 GALLON
LEYMUS CONDENSATUS 'CANYON PRINCE'	CANYON PRINCE WILD RYE	3'-4'	3'-4'	5 GALLON
MISCANTHUS SINENSIS ADAGIO	DWARF MAIDEN GRASS	12"	3'-4'	1 GALLON
MUHLENBERGIA RIGENS	DEER GRASS	12"	3'-4'	1 GALLON
SENECIO TALINOIDES VAR. MANDRALISCAE	BLUE CHALK STICKS	12"	3'-4'	1 GALLON
TULBAGHIA VIOLACEA	SOCIETY GARLIC	1'-2'	2'-3'	1 GALLON

TREES:

BOTANIC NAME	COMMON NAME	HEIGHT*	WIDTH*	SIZE
KOELREUTERIA PANICULATA	GOLDEN RAIN TREE	30'-40'	30'-40'	48" BOX
PARKINSONIA ACULEATA 'DESERT MUSEUM'	DESERT MUSEUM PALO VERDE	20'-30'	20'-25'	36" BOX
TIPUANA TIPU	TIPU TREE	40'-50'	35'-40'	48" BOX
PHOENIX DACTYLIFERIA	DATE PALM	40'-50'	-	18'-25' BTH
WASHINGTONIA ROBUSTA	MEXICAN FAN PALM	50'-90'	-	18'-25' BTH

PLANTING PALETTE FOR AREAS WITH WATER INDUNATION: NORTHERN SEGMENT



CAREX DIVULSA



DALEA CAPITATA 'SIERRA GOLD'



JUNCUS PATTENS



MISCANTHUS SINENSIS ADAGIO



MUHLENBERGIA RIGENS



TULBAGHIA VIOLACEA

PLANTING PALETTE FOR AREAS WITHOUT WATER INDUNATION: NORTHERN SEGMENT



AGAVE AMERICANA



AGAVE ATTENUATA



CEANOTHUS JOYCE COULTER



DRACAENA DRACO



AGAVE ANGUSTIFOLIA VAR. MARGINATA



ALOE STRIATA



DIETES GRANDIFLORA



HELICOTRICHON SEMPERVIRENS



JUNCUS PATTENS



LEUCOPHYLLUM FRUTESCENS



MUHLENBERGIA RIGENS



SENECIO TALINOIDES VAR. MANDRALISCAE



KNIPHOFIA 'ECHO ROJO'



LEYMUS CONDENSATUS 'CANYON PRINCE'

*Note: A planting palette is provided for each segment, for areas with water inundation. Planting in these areas are expected to be submerged, or partially submerged, during stormwater events, in areas that will be used for stormwater conveyance, and as part of LID solutions.

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4.5 TREATMENT OF RESIDENTIAL BACKYARD WALLS

Portions of the corridor are “one sided” as they include parcels with downhill slopes that address Western Avenue. In these areas, there is little opportunity to change the character of the street edge beyond the right-of-way. Three solutions are identified, below and right, providing guidelines for aesthetic improvements to the existing retaining wall.

Option A

1. Consider aesthetic and/or surface treatment to the existing wall.

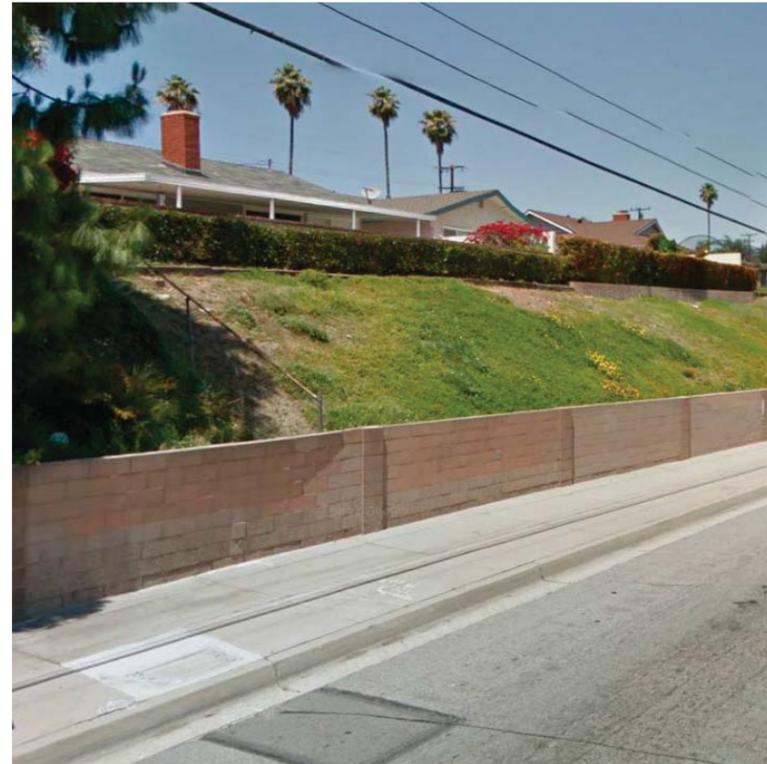
Option B

1. Consider painting the wall, and the addition of vine pockets along the sidewalk.
 - In this option, care should be taken to not reduce the width of the pedestrian way.
2. As an alternate solution, the City, in combination with Caltrans, should consider the use of a landscape easement, of approximately 3 feet, west of the wall.
 - The landscape easement could be used to soften the wall edge. Prostrate rosemary, grasses, and/or other hanging plant material could be used to cascade over top edge of retaining wall without impeding on sidewalk pedestrian width.
 - In this solution, a temporary irrigation drip line should be used for the establishment of water-wise plant material.

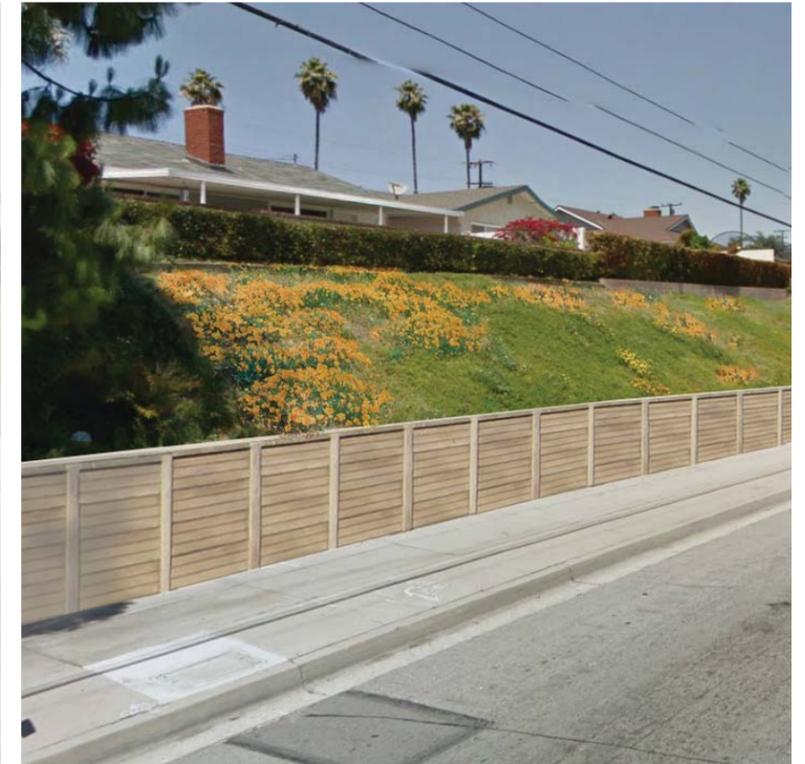
Option C

1. Consider the addition of a split-rail fence, with a narrow planting buffer, to the east of the wall.
 - In this option, care should be taken to not reduce the width of the pedestrian way. This option may not be suitable without additional changes to the right-of-way.

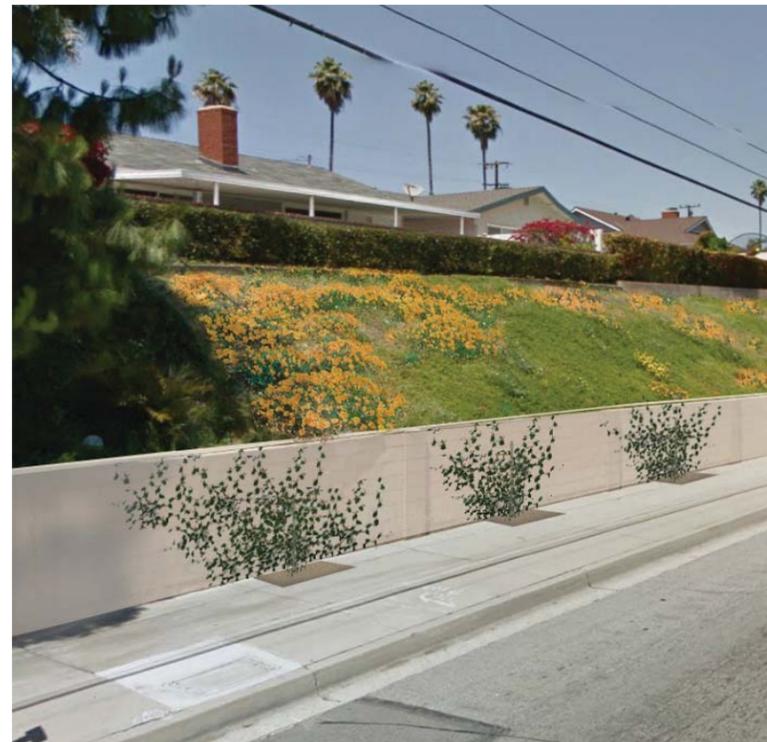
There may be opportunities for the City of RPV in conjunction with the City of LA and Caltrans, to assist homeowners with improvements to residential slopes and backyard walls. For example, free water-wise plant materials could be provided, or community events could be organized to purchase and/or install planting materials or trees. Additionally, opportunities may be considered to utilize the wall for public art.



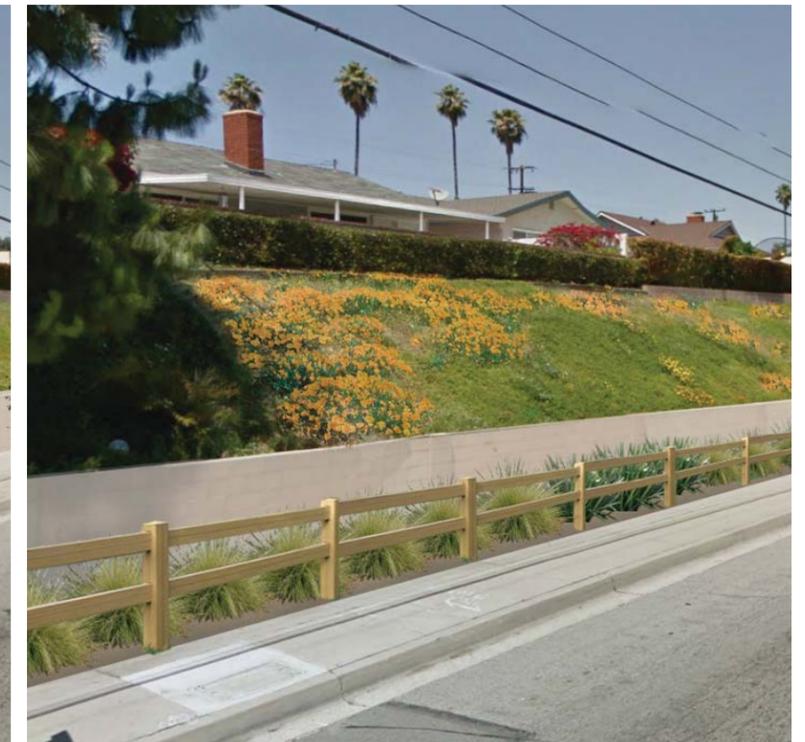
Existing cinder block wall along residential backyards.



Option A: Faux wood-grain panels attached to existing wall, enhanced slope planting.



Option B: Painted wall, vine pockets along sidewalk, enhanced slope planting.



Option C: Split-rail fence, painted wall, narrow planting buffer, enhanced slope planting.

4.6 BRANDING, SIGNAGE, AND WAYFINDING

The design and character of signage and wayfinding should create a distinguishing design theme and brand for Western Avenue. [Discuss the inclusion of the program; if included add text to the following effect: A District Signage and Wayfinding Program should be developed in conjunction with the streetscape design to support the consistent implementation of the Guidelines within both the Cities of LA and RPV. The Program should further specify and detail the size, type, design, and location for the installation of all individual components.]

General Signage and Wayfinding Guidelines

The following guidelines should be followed for the design of all signage and wayfinding in the project area. These include signs of all types, and for all audiences. The guidelines do not supersede the requirements of the zoning code, rather they provide additional design direction specific to the goals for the corridor.

Any project incorporating two or more signs within the project area will be required to submit a Sign Program during the design development phase. The Sign Program shall identify all types of proposed signs included in the project that can be viewed from the street, sidewalk, or public right-of-way. All signs are subject to the Design Review Process of the respective City. All signs that project into the public right-of-way must also be reviewed by the City Engineer of the respective City. Additionally, all sign lighting shall comply with light pollution reduction standards.

1. Signage and wayfinding should work together to create a Western Avenue brand and identify, and should not create visual clutter along Western Avenue.
2. The signage color, material, scale, lettering, and lighting should complement the surrounding street environment and the building(s) that the sign addresses.
3. Signs should never overpower the building or project.
 - Signage should fit comfortably into the architecture of the storefront.

- The height, location and size of a sign should not obscure visibility into the site or storefront active use of the space.
4. Information on a sign should be brief, clear, and simple, with appropriately sized lettering, and a clear information hierarchy. When appropriate, symbols or logos can be used in place of text.
 5. One business sign should be installed per building frontage.
 - Redundant signage should be avoided.
 - If multiple tenants are listed on a single sign or a multi-tenant building, variation between size and typeface of tenant names and color palette should be limited to one or two options.
 6. Minimal lighting should be used for signage and light pollution should be avoided. Additionally, lighting should be in scale with the size of the sign and the facade.
 7. Signage design should convey a timeless character of the street, place, or business.
 8. Signage should be designed with durable materials and be well maintained.
 9. [The following text should be discussed with City staff: The following signage types are not permitted in the project area.
 - Pole signs
 - Signs obscuring windows
 - Neon, animated, or flashing signs
 - Internally illuminated awnings
 - Signs projected onto a surface using light
 - Inflatable or air blown signs, streamers, balloons, and the like
 - Signs illuminated by low-pressure sodium lamps (pure yellow glow), high pressure sodium lamps (pinkish-orange glow), and mercury vapor lamps (bluish-white glow)]

Guidelines for Pedestrian-Oriented Signage and Wayfinding

1. The District Signage and Wayfinding Program should identify one to two types of pedestrian-oriented signs, for

consistent use along the corridor, at the following locations:

- Along Peck Park
 - Crestwood intersection
 - Capitol Drive intersection
 - Caddington Drive intersection
 - Westmont Drive intersection
 - Montgomery Drive intersection
 - Along Green Hills Memorial Park (Cemetery) and the San Pedro Defense Fuel Support (DFSP)
2. The signage should incorporate the community logo, and be of a material, font, color, and design that is complementary to other streetscape elements throughout the corridor.
 3. The use of in-grade [identity] is encouraged as part of the branding of the Western Avenue Corridor.
 - The signage should include the Corridor logo, and may add additional wayfinding information such as direction, mileage markers, year implemented or date of founding, etc.
 - Additionally, this type of signage may be incorporated into the design of streetscape components, such as seating, trash receptacles, newspaper racks, street signs, park signage, etc.

Guidelines for Gateway Signage

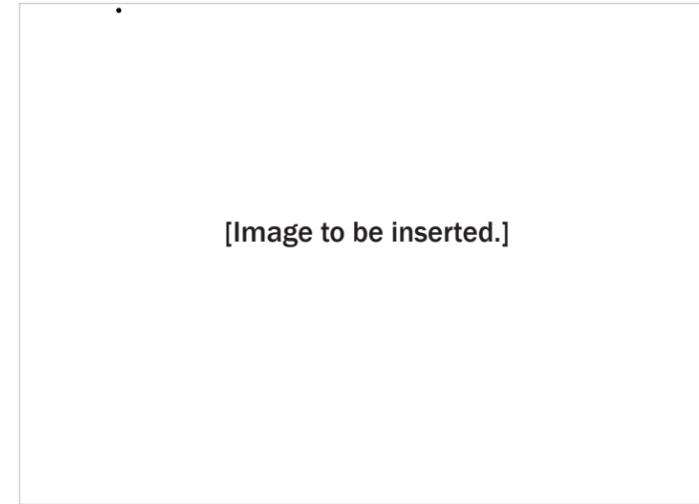
1. The location for Gateway Signage is identified in [Figure X-X] on [Page X].
2. Gateway Signs may include the following sign types:
 - Signs located in the median
 - Arch-type signs spanning the width of the corridor and mounted either in the median, or at each side of the parkway.
 - Groupings of signs designed together, [explain]



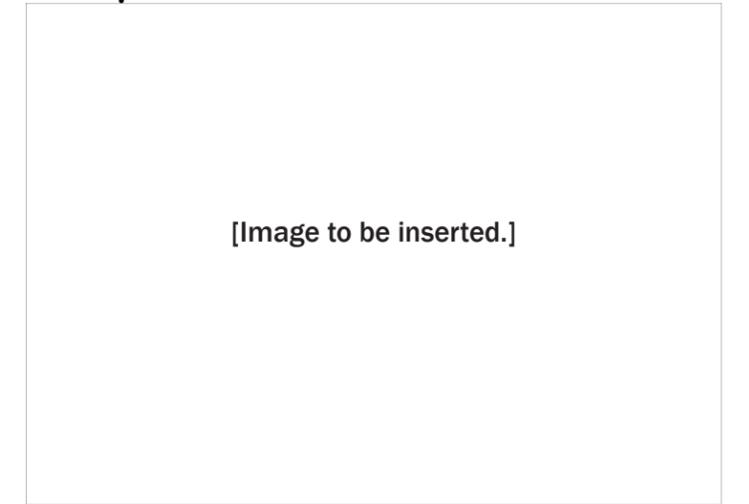
Western Avenue at Summerland Avenue, view north



Western Avenue at Caddington Drive, view south



Western Avenue at Caddington Drive, view north



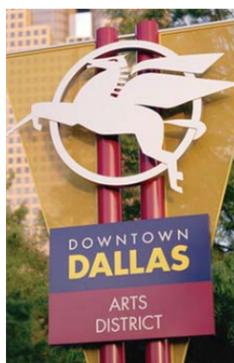
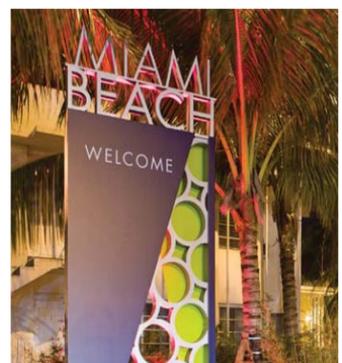
Western Avenue at Palos Verdes Drive, view south



SIGNAGE: Entry Monuments and Vehicular-Scaled Gateway Signage



Linear signs can be integrated with landscaping and lighting.



Median-supported signs can make a big statement when parkway dimensions are limited.

Gateway signage should be designed in character with the branding of the district.

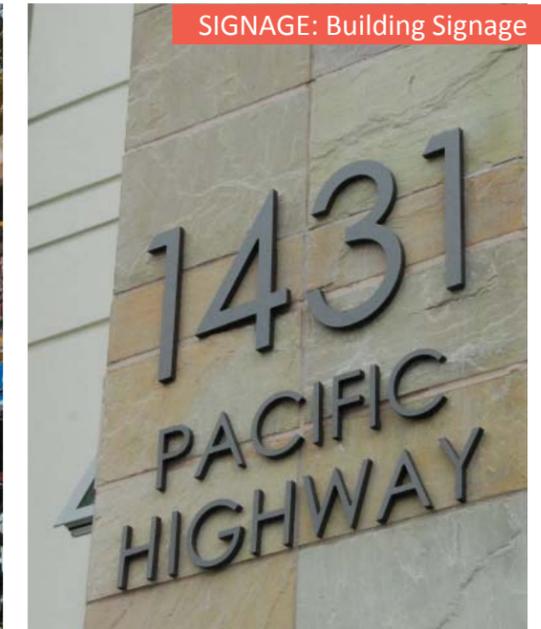
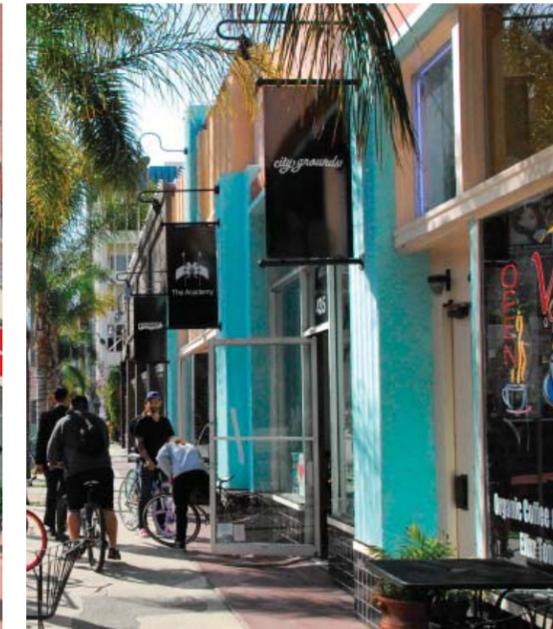
Branding, signage, and wayfinding can be integrated with streetscape elements for a cohesive design character.

[GENERAL NOTE: This page is shown for discussion purposes only, and may be eliminated in the final document.]



SIGNAGE: Pedestrian-Scaled Signage and Wayfinding

Pedestrian-oriented signage should incorporate the community logo, and be of a material, font, color, and design that is complementary to other streetscape elements throughout the corridor.



SIGNAGE: Building Signage

Signage color, material, scale, lettering, and lighting should complement the surrounding street environment and the building(s) that the sign addresses.

[GENERAL NOTE: This page is shown for discussion purposes only, and may be eliminated in the final document.]

4.7 Public Art

The Northern Segment of the Western Avenue corridor can benefit from public art, whether monumental and permanent, or ephemeral and temporary. With a wide street width and restricted space within the pedestrian right-of-way, this portion of Western Avenue lends itself to median located features, choosing from a variety of branding elements: vertical, repeated, sculptural art, iconic markers, entry monuments, and more. Alternately, with participation by the Green Hills Memorial Park, and the San Pedro Defense Fuel Support (DFSP), public art could be located within the setbacks of those properties.

1. A Public Art Program should be created to support the implementation, installation, and maintenance of Public Art pieces.
2. Public Art is envisioned at the following locations:
 - Monumental-scaled public art installation at the northern segment of the corridor, located in the public right-of-way, and/or the Green Hills Memorial Park (Cemetery), and the San Pedro Defense Fuel Support (DFSP).
 - Public art is encouraged along the corridor, and at public spaces such as public parks, plazas, greenways, paseos, and other open spaces.
 - The design and installation of Public Art should be planned in conjunction with the design of the public realm, and coordinated with the installation of street furniture, utilities, landscape, and planting.
3. Public Art is encouraged in privately owned developments.
 - Artwork in privately owned developments should be fully integrated into the development's design, in the most accessible and visible locations. For example, enclosed lobbies and roof top gardens are considered appropriate locations.

Examples of on-street engaging landscape, roadside art, entry signage and monuments, that may provide inspiration for a Western Avenue Public Art Program.

CO2LED PUBLIC ART INSTALLATION, VIRGINIA

A group of artists created this temporary public art project aimed at raising awareness of global warming. The project, called CO2LED, is made up of more than 500 plastic water bottles attached to white plastic poles ranging from 5 to 13 feet high. Inside each inverted water bottle is a bright white LED light. At a distance, the stems look like gently bobbing cattails in the median of a busy intersection. The high-efficiency LED lights are lit by solar power.



LAX GATEWAY MONUMENTS, LOS ANGELES, CA

The LAX Gateway Monuments, as an art piece, signifies one's entry or exit from the City of Los Angeles. While a gateway monument of this size many not be appropriate for Western Avenue, the idea of having some sort of gateway or entry signage is appropriate. The northern segment is



ENCHANTED HIGHWAY, NORTH DAKOTA

The Enchanted Highway, a 32 mile stretch of highway through open country land in North Dakota, features some of the world's largest scrap metal art sculptures. Monumental public art can be similarly displayed along the northern segment of Western Avenue. This segment is also an ideal location for a gateway element announcing one's arrival into the commercial heart of the study corridor.



[Image to be inserted.]



5

Administration

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Purpose of the Design Guidelines

The Western Avenue Design Implementation Guidelines (Guidelines) are intended to move the Western Avenue Vision Plan closer to implementation by providing guidelines for streetscape improvements and urban design, for properties along the corridor in the project area.

[Add statement, to be confirmed with Caltrans, to the following effect: The Western Avenue Corridor will be implemented as a Caltrans “Main Street” pilot project and will continue to be maintained and operated by Caltrans.]

The guidelines established in this document apply to all projects and improvements, including both public rights of way improvements and private development projects, within the project area identified on Figure X-X, in both the City of Rancho Palos Verdes (RPV) and the City of Los Angeles (LA).

The Western Avenue Design Implementation Guidelines are intended to be implemented separately in the City of RPV and the City of LA. These guidelines do not revise existing land use or zoning policies, but instead aim to shape future developments and improvements to be consistent with the established vision and within the existing regulatory framework.

Applicability to the City of Rancho Palos Verdes

How the Guidelines are applied

[This section is pending feedback from the City of RPV]

How to use the Guidelines

[This section is pending feedback from the City of RPV]

Relationship to the City of Rancho Palos Verdes General Plan, Zoning Code, and Specific Plans

[This section is pending feedback from the City of RPV]

Applicability to the City of Los Angeles

How the Guidelines are applied

The Guidelines are intended for the Planning Department, as well as other City agencies and department staff, developers, architects, engineers, and community members, to use in evaluating project applications along with relevant policies from the General Plan Framework and Community Plans. To achieve the stated purpose, the Guidelines will apply to all new developments and substantial building alterations that require approval by decision-making bodies and planning staff. However, all “by-right”¹ development projects are also encouraged to incorporate the Design Guidelines into their project design.

Each of the Western Avenue Design Guidelines should be considered in a proposed project, although not all will be appropriate in every case, as each project will require a unique approach. The Western Avenue Design Guidelines provide guidance or direction for applying policies contained within the General Plan Framework and the Community Plans. Incorporating these guidelines into a project’s design will encourage compatible architecture, pedestrian activity, context-sensitive design, and contribute to placemaking.

How to use the Guidelines

Property owners, developers, designers, and contractors proposing new development in Los Angeles should first review the zoning of the property being developed. They should then proceed to the Western Avenue Design Guidelines appropriate to the project.

The provisions set forth in this document identify the desired level of design quality for development. However, flexibility is necessary and encouraged to achieve excellent design. Therefore, the use of the words “shall” and “must” have been purposely avoided within the specific guidelines. Each application for development, however, should demonstrate to what extent it incorporates these guidelines.

Applications that do not meet specific guidelines applicable to that project should provide rationale for the design and explain how the project will meet the intent of the General Plan, the Municipal Code, and these Guideline objectives. Whether the design is justified will be determined through required “Findings” in the appropriate section of the Los Angeles Municipal Code.

Relationship to the City of Los Angeles General Plan, Zoning Code, and Citywide Guidelines

The approval process for new development is guided by the General Plan, Chapter I of the Los Angeles Municipal Code, and the Citywide Design Guidelines.

City of Los Angeles General Plan: Comprised of 35 Community plans, the General Plan is the policy document that sets the development vision of the community. It provides policy direction for land use, vehicular and bicycle circulation, open space and recreation, and infrastructure.

Los Angeles Municipal Code: Adopted ordinances that implement the General Plan by establishing land use and development requirements. The Municipal Code includes provisions for the establishment of specific plans and supplemental use districts.

Citywide Design Guidelines: Establishes best practices for designing high-quality development that meets the objectives of the General Plan. Certain items apply to site planning and others to building design and aesthetics. This document applies to all areas, but is particularly applicable to those areas within the City that do not currently have adopted design guidelines.

¹By-right: Projects which meet all LAMC zoning regulations and require review only by the Department of Building and Safety.



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Acknowledgments

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